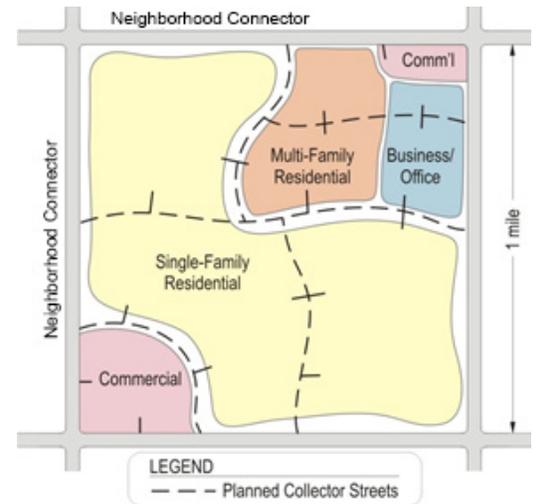


# COLLECTOR STREET PLANNING

**WHAT ARE COLLECTORS?** The tributaries of the local transportation network, collectors provide critical connections throughout the network and bridge the gap between local streets and the thoroughfares of a community.

## BENEFITS

- Promote street connectivity
- Provide connections between thoroughfares
- Connect adjacent neighborhoods
- Facilitate efficient dispersion of traffic
- Provide opportunities for bicycling and walking
- Promote reasonable street spacing
- Anticipate/facilitate effective future street connections



## TYPICAL CONTEXT

Collector design in Fort Worth is a careful balance between providing direct connectivity and attracting no more traffic than is appropriate.

	Residential areas		Non-residential / mixed-use areas
	With fronting single-family homes*	No fronting single-family homes	
Typical trip length	≤ ½ mile	≤ 1 mile	up to 2 miles
Upper limit daily traffic volume (both directions)	2,000	5,000	10,000
On-street parking	Required	Allowed but not required	Allowed but not required

\* Collectors without fronting homes are preferred.

**DESIGN APPROACHES** Design features must strongly encourage speeds of 25 mph or less and should provide visual cues to drivers that the street is not intended for long-distance trips. Several techniques that can be considered:

### Curvilinear street design

Promoting low speed and increased driver



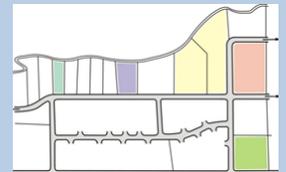
### Neighborhood entry features

Visually discouraging cut-through traffic by identifying as a neighborhood street



### Subdivision layout

Designing the network to achieve the desired balance between speeds and traffic flows



### Traffic calming measures

Additive design features to slow traffic (roundabouts, on-street parking, etc.)



### Network discontinuities

Designing offsets, gaps, and L-shaped streets to discourage cut-through (not first preference)



**CONNECTIONS AND SPACING** If collectors are spaced too far apart, they begin to lose their value as connections for the overall network. Depending on the setting, Fort Worth prescribes a set of desired **maximum spacings** to promote network connectivity.

Land Use		Dwelling Units/Acre	Access Function	Desired Maximum Spacing between Collector Intersections along a Thoroughfare (feet)
Residential	Rural	< 2	N.A.	N.A.
	Suburban	2-4	High	1,500 – 3,000
	Urban	>4	High	750 – 1,500
Non-Residential & Mixed-Use		N.A.	Medium	750 – 1,500

Additional connectivity considerations:



Collectors must include **pedestrian facilities** on both sides.



Using collectors as connecting links in the city's **bicycle network** is highly encouraged.

Collectors must only **terminate at an intersection with a thoroughfare or another collector** (unless prevented by natural barriers or stubbed for a future extension).

Collectors must connect to thoroughfares at **full median opening** locations.

Where feasible, collector/thoroughfare connections should be made at **signalized intersections** or locations suitable for future signals.

## COORDINATION

With respect to collectors, development reviews must consider the transportation context of all other plans – and existing developments – in the thoroughfare-bounded area that includes the proposed development. A new development's street system must include appropriate contributions and connections to a successful collector system.

The ideal method for ensuring proper collector spacing and function is to develop a **small-area collector plan**, when resources, property ownership conditions and timing make it possible. Such a plan illustrates the ultimate collector network in an area bounded by thoroughfares.

Even if such a plan is not prepared, several collector-related considerations should be part of each planned development's roadway network:

- A network of on- and off-site connections and roadways to allow for movement between destinations without using the thoroughfare network.
- Connection with stub streets.
- The potential for a collector as a boundary between adjacent land uses of differing types (e.g., residential vs. commercial).
- The preference to align offset intersections across thoroughfares (unless cut-through traffic is a major concern).
- The need to coordinate with adjacent agencies at jurisdictional boundaries.