



SOUTH MAIN URBAN VILLAGE MASTER PLAN

DECEMBER 2007

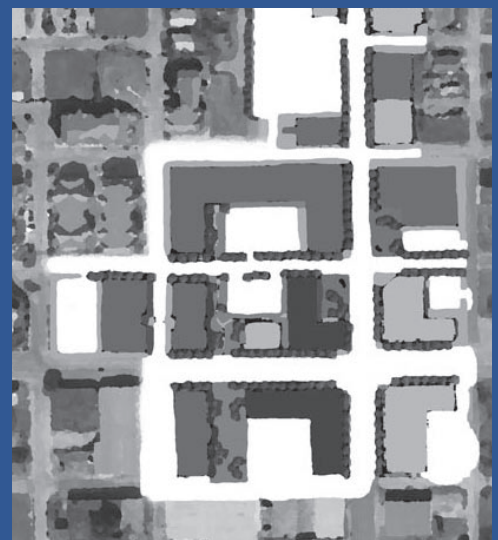


TABLE OF CONTENTS

1. Introduction	1
1.1 PLAN PURPOSE	1
1.2 PLAN PROCESS	1
2. Master Plan Principles	3
2.1 HISTORICAL CONTEXT	3
2.2 EXISTING CONDITIONS	4
2.3 VALUES & PRINCIPLES	5
3. Urban Village Plan	7
3.1 MAIN STREET CORE	8
3.2 FORT WORTH SCHOOL DISTRICT SITE	10
3.3 TRANSIT-ORIENTED DEVELOPMENT	10
3.4 URBAN VILLAGE RESIDENTIAL/ OPEN SPACE	11
3.5 SOUTH MAIN/VICKERY BOULEVARD- IMPROVEMENTS	12
4. Implementation	15
4.1 POLICY TOOL BOX	15
4.2 MARKETING STRATEGY	15
4.3 ACTION PACKAGE	20

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SUMMARY

The Urban Village Master Plan document sets out a short and long-term vision for the village. This vision is the product of three public work sessions, one-on-one interviews and nearly a year long sequence of activities, including initial analysis, staff conversations, establishment of principles and values, and subsequent development of concepts, strategies, and projects.

The chapters are organized to build upon the decisions and strategies noted in previous chapters. For example, early in the process, the plan included a work session in which perceptions and preferences were gathered and recorded, as well as priorities for transportation improvements. The comments and site analysis served as the basis for the formulation of vision principles and values, which are discussed in the early chapters. Based upon these principles, detailed strategies, catalyst projects, and implementation processes are developed in subsequent chapters.

The level of specificity also increases in follow-up chapters. For example, the issues and public comments in Chapter 1: Introduction, and the general vision principles and values noted in Chapter 2: Master Plan Principles, are interpreted and become the basis for the Plan shown in Chapter 3. The outgrowths of Chapter 3: The Plan are specific items outlined in Chapter 4: Implementation, which sets out the policy, actions, and catalyst projects to achieve the completion of the South Main Urban Village Plan.

Consistent with the above approach, the chapters are summarized as follow:

CHAPTER 1: INTRODUCTION

This chapter describes the general approach to the master plan, including how the document is organized, and the comments gathered as part of the public work session process. It also provides a brief history of the Urban Village initiative and South Main today.

CHAPTER 2: MASTER PLAN PRINCIPLES

The master plan principles reflect the concerns which were established as a result of site analysis, the public work sessions, and other reviews. These principles affirm the short and long-term direction of the master plan, and the essential philosophies which should guide development.

CHAPTER 3: URBAN VILLAGE PLAN

The core of this chapter consists of five specific directives that fulfill the master plan principles. Four catalyst project areas form the foundation of the master plan. In addition, a fifth area of focus is the prioritization of transportation and infrastructure improvements as part of the state transportation funding grant.

CHAPTER 4: IMPLEMENTATION

The roles of city leadership, neighborhood associations, businesses, and property owners are key to the success of the plan. This chapter provides a partnership tiered approach, including policies, actions, and how they apply to catalyst projects.

APPENDIX

This section contains the following information:

1. Best practice examples.
2. Market and demographic data.
3. Historic inventory data.
4. Crime trends.
5. Near Southside development standards and guidelines.
6. Public meeting summaries.

1. INTRODUCTION

1.1 PLAN PURPOSE

Why create an urban village master plan? - Since early 2001, the City of Fort Worth has embarked on creating dynamic urban villages across the City core. South Main, with its proximity to downtown and the medical district, is a natural draw for future investment. With the recent construction of downtown loft housing and growth at the current TRE station, South Main’s historic infrastructure and vacant land bring many opportunities and challenges for the future.

There are few documents which are more comprehensive or informative than an effective master plan. The process of framing such a plan places a premium on gauging needs of various kinds: the assessment of existing facilities, the demand for new ones, the response to experienced growth, and the anticipation of future changes. It also facilitates the adjustment of attitudes and trends. The comprehensive approach to growth used by Fort Worth South in the past and at the present, only gives this planning tool for South Main more importance.

How to approach the master plan? – The creation of a useful master plan requires understanding the nature of an urban village, making calculations regarding its future needs, and setting out a method of satisfying them. For South Main Urban Village, the master planning effort requires a balancing of its traditions with an informed inquiry into its future. A master plan should benefit from the village’s ample experience related to the planning and operation of the village over the course of a century while absorbing successive generations of demographic, market, and physical changes. This experience gives the plan an appropriate context, reflective character, and historical perspective.

What were the key goals for the urban village in embarking upon the plan? – As articulated by City leadership, South Main residents, and property owners, the key objectives for the urban village master plan process are:

- Vision consensus.
- Partnership framework.
- Development opportunities.
- Main Street revival.
- Practical solutions.
- Transportation improvements.

1.2 PLAN PROCESS

The master plan was a 10-month long effort designed to seek input and ideas from all stakeholders who may have an interest in the urban village future. The eight-task process included three public work sessions, conversations with City staff and village advisory team members, and one-on-one interviews.

The process was organized in a series of tasks:

Task 1.0	Project Start-up and Project Management
Task 2.0	Public Involvement
Task 3.0	Project Context, Inventory and Analysis
Task 4.0	Development Opportunities
Task 5.0	Transportation Needs and Priorities
Task 6.0	Market Analysis
Task 7.0	Land Use Analysis
Task 8.0	Urban Village Master Plan

2. MASTER PLAN PRINCIPLES

This chapter identifies village-wide values that constitute the framework for the plan. Historical context, values and principles, and best practice examples make up the basis for the South Main Urban Village Plan.

2.1 HISTORICAL CONTEXT

The South Main Street area was one of Fort Worth's most successful areas for commerce. It's strategic location along the rail lines and proximity to downtown gave this commercial area a competitive advantage. The area evolved into a main street of activity until interstate highway barriers were erected.

As the City grew outwards, South Main Street began to change. Although medical facilities began developing to the west and south, the South Main Street corridor began to be riddled with crime. Buildings and

structures were eventually demolished and land sat vacant.

When Fort Worth South, Inc. organized, an aggressive effort for urban renewal resumed. At the same time, the area's homeless population increased, with the nearby shelters attracting homeless individuals and families from throughout the region. Soon, an organized day labor operation became the primary tenant at the corner of Vickery Boulevard and South Main Street.

However, recent investments and developments have brought new hope for the area. The Lancaster Avenue road re-alignment has created a new demand for mid-rise residential living along Interstate 30. In addition, the new commuter rail routes and bus station at Vickery and I-30 provide new opportunity for transit-oriented development within the South Main Urban Village boundaries.



Existing conditions along South Main Street.



The existing transportation network affords South Main excellent visibility and mobility.



The transient population presents challenges that require positive solutions.

2.2 EXISTING CONDITIONS

First impressions of any neighborhood, commercial corridor, or community go a long way in determining long term perceptions of an area. Today, South Main has many opportunities and challenges related to current conditions. The South Main Urban Village is roughly defined by Interstate 30 (north) and Interstate 35 (east), Rosedale Street (south) and Hemphill Street (west).

The village has excellent visibility to vehicular traffic as well as natural advantages with its proximity to downtown. Multi-modal connections via the Trinity

Railway Express (TRE) station provide rail, bus, and vehicular options. The existing grid street alignment and vacant parcels allow for significant infill investment. The strongest nucleus of South Main historic structures runs along South Main Street from Vickery Boulevard to Peter Smith Street.

Vacant and boarded up structures exist throughout the village with little residential population. The existing businesses within the village rely very little on the local market. Utilities for the village are generally aged and inadequate for major redevelopment requirements. The largest single developable property (7.5 acres) is currently owned by the Fort Worth Independent School District and located at the Pennsylvania Avenue and South Main intersection.

Gateway opportunities exist along South Main Street at the intersections of Vickery and Rosedale. Most of the vacant parcels are located east of South Main Street on both sides of Hattie Street. A high level of pedestrian traffic concentrates along Vickery Street between the homeless shelters, the day labor building, and Broadway Baptist Church.

During the first public work session, citizens agreed on the following opportunities and challenges.

OPPORTUNITIES

- Historic Fabric
- Transportation Access
- Mobility
- Proximity to Downtown
- Medical/Health Care Base
- Visibility

CHALLENGES

- Transient Population
- Public Safety
- Vacant Parcels
- Lack of Services, Retail, Housing
- Streetscape
- Parks & Open Space

A visible transient, labor, and homeless population congregates throughout the urban village area. Most of the pedestrian traffic from these groups occurs between the Baptist church, labor center, and homeless shelters across the tracks. As part of the City's overall strategy to combat homelessness, the Mayor established an advisory committee and charged the group with preparing a ten-year plan to end chronic homelessness.

Several housing strategies are already in place to assist in the transition of the homeless population. These include the Housing First Program and Rapid Re-Housing (Housing Trust, dispersion and inclusionary housing). In addition, the City has implemented a Neighborhood Improvement and Public Safety area that establishes no alcohol zones. A successful South Main Urban Village transformation will only be possible if the homeless and transient populations are comprehensively addressed.

2.3 VALUES & PRINCIPLES

Based on historical information and public work session consensus, a new South Main Urban Village vision emerged. This vision is dynamic and based on key values and principles. Images from other similar urban areas were also identified and ranked as part of the visioning exercise. The values and principles agreed upon during the public work sessions include:

- Historic Main Street fabric
- Multi-modal transportation access
- Proximity to Downtown culture
- Infill opportunities
- Medical/healthcare employment
- Wellness theme
- Positive solutions on homeless populations
- Poster child for urban village incentive zone in attracting private investment

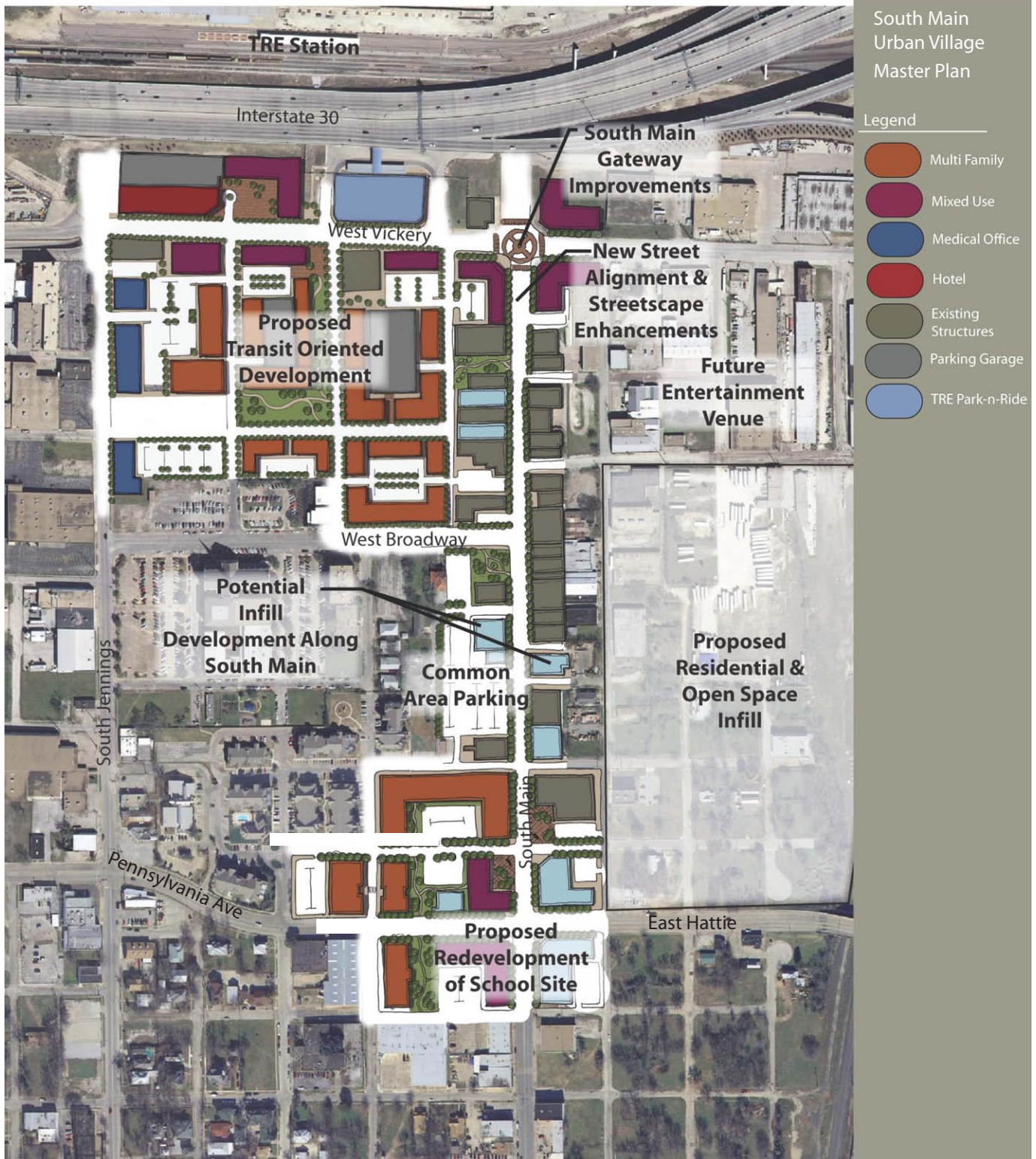


During public workshops, residents expressed a desire for a pedestrian-friendly main street with a mix of land uses.



Private investment where infill opportunities exist will add vitality to the urban village.

3. URBAN VILLAGE PLAN



3.1 MAIN STREET CORE

The South Main Urban Village consists primarily of commercial, institutional, and vacant land uses. Very little residential exists in the area, with the exception of some multifamily. Because of the large amount of vacant building space and land, the primary focus of the village plan should begin in the core of the urban village on South Main Street between Interstate 30 (north) to Pennsylvania Avenue (south).

The strategy along South Main Street is two-fold, addressing both street improvements and redevelopment. The number one priority for the entire urban village is the reconstruction and alignment of South Main Street. The first phase of the capital project should extend from the interstate to Pennsylvania. If resources can be obtained, the capital project should extend south to Rosedale Street.

South Main Street has already been targeted for a 12-inch waterline replacement project from Vickery to Dashwood. Although construction funds have not been identified, South Main Urban Village leaders and stakeholders should rally around the reconstruction/redesign of South Main simultaneously to leverage other funding sources; such as a future City bond initiative, state highway enhancement dollars, and other sources.

The City of Fort Worth heard loud and clear from public work sessions that the current design of South Main Street allocates too much right-of-way to vehicle travel lanes and not enough for urban village level pedestrian activity. Sidewalks are too narrow and inconsistent, and the street is too wide with very little defined street parking. The streetscape today is non-existent and the street is not taking advantage of a natural “main street” character on the upper end of South Main near the Vickery intersection.

The current right-of-way width of South Main Street is adequate to allow for a reconfiguration of traffic lanes and on-street parking. The City should undertake a comprehensive redesign of South Main Street to allow for continuous parallel parking on both sides of

the street, with two moving lanes and one continuous turning lane. The turning lane would serve an existing purpose to allow turns at intersections, but should also be designed to accommodate future light rail use serving the village, downtown, TRE Station, and medical campus.

A redesign of South Main Street from Interstate 30 to Rosedale would allow for a more unified pedestrian-friendly streetscape, providing more on-site parking for adjacent businesses and wider sidewalks for pedestrian use. Other improvements as part of the South Main Street Enhancements include:

- Street lighting
- Street banners
- Textured sidewalks
- Pedestrian crossings
- Appropriate landscaping
- Parking enhancements

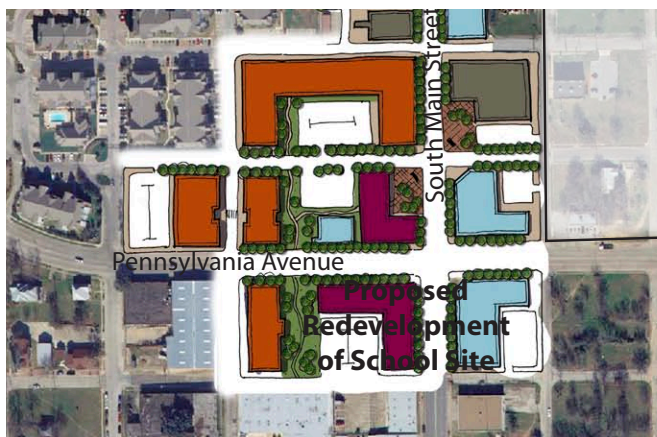
In addition to the street redesign and streetscape enhancements, the second primary strategy along the South Main Street core area should focus on increasing occupancy of existing buildings, appropriate infill of vacant parcels, and façade rehabilitation of existing buildings.

The total building square footage available within the South Main Street core today, from Vickery Boulevard to Annie Street, consists of approximately 305,000 square feet. First story space is approximately 255,000 square feet. The total occupancy rate today is approximately 47%. The goal for the South Main Urban Village, within this defined core area, should be 80% occupancy by the year 2015, assuming redesign and reconstruction of South Main Street is complete.

New infill structures and existing façade improvements should also be made in accordance with the current scale and “Main Street” design. The Near Southside Development Standards and Guidelines should be used as a framework for urban design.



Enhancements to South Main Street will create a vibrant, pedestrian-friendly environment.



The school district site is appropriate for light industrial or mixed-use development.



Transit-oriented development will draw a mix of uses to the urban village.

3.2 FORT WORTH SCHOOL DISTRICT SITE

Perhaps the largest parcel of developable land which fronts South Main Street is a 7.5-acre parcel owned by the Fort Worth Independent School District. The property is located west of South Main Street on both sides of the intersection at Pennsylvania Avenue. The property was used for light industrial use and is now for sale as one unit by the school district.

This particular development site is most important to the future direction of the urban village. The site is

conducive as either light industrial or mixed-use. The City should make an aggressive effort to attract a large employer, preferably a medical or health care use, which would employ a large number of individuals that would then frequent businesses along South Main Street. A large employer would also create a new demand for residential nearby. A mixed-use development combining a large employer with ground floor retail would also be ideal for this strategic site.

3.3 TRANSIT-ORIENTED DEVELOPMENT

Perhaps the greatest short and long term opportunity for the South Main Urban Village is the transportation network that surrounds its core. The relationship to downtown and proximity to new residential and convention facilities focuses new attention in the South Main core. Highway traffic from Interstate 30 and 35W gives the village greater exposure and accessibility.

Even of greater importance, however, is the existing TRE station north of Vickery at Interstate 30. Currently, the TRE provides commuter rail service to the City of Dallas. Future plans will connect this station to DFW airport. This “hub” of transportation options in the shadows of downtown and a major interstate system, create a unique opportunity for a transit-oriented development (TOD) along Vickery at South Main Street and south into the urban village.

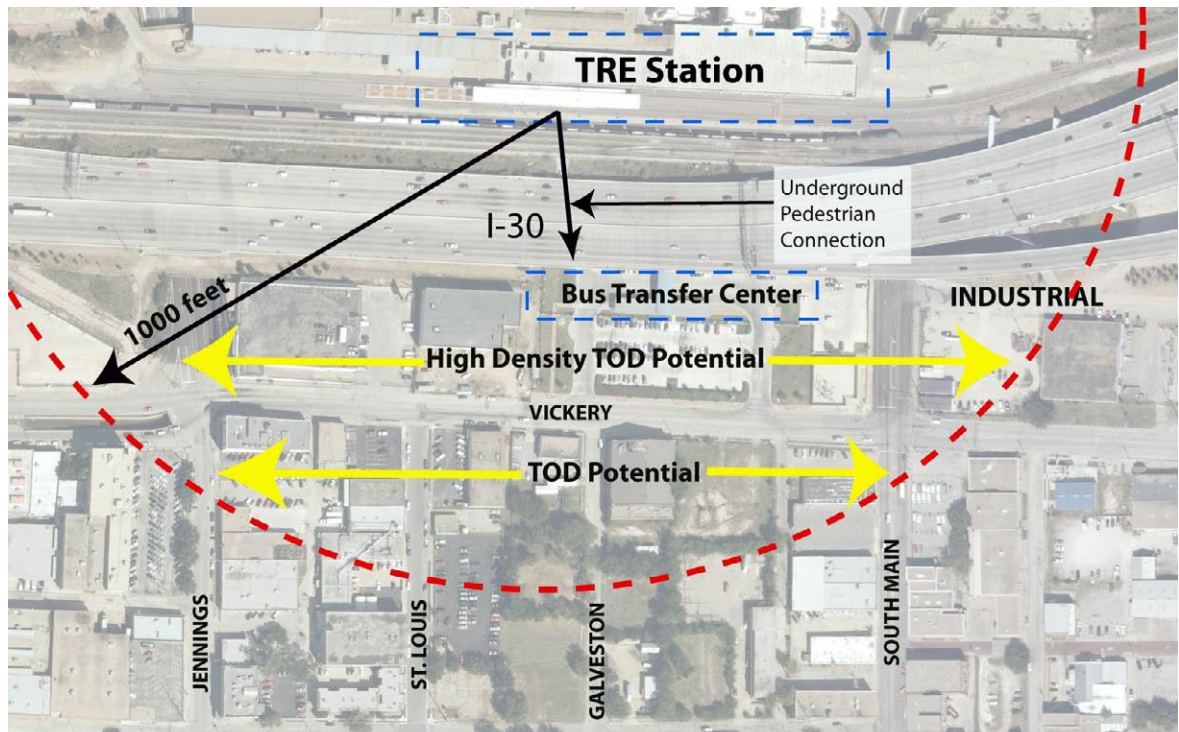
The TOD is an excellent magnet for mixed-use developments. Parcels north of Vickery from Jennings to the rail crossing become ideal locations for mid-rise residential and hotel uses. Careful attention to downtown views and scale should be considered as this demand evolves. In addition, adaptive re-use of historic structures, especially between Jennings and St. Louis, should be considered as part of mix-use development. Other vacant or underutilized structures should also be candidates for TOD mixed-use development.

3.4 URBAN VILLAGE RESIDENTIAL/OPEN SPACE

An important complement to mixed uses along the South Main Urban Village core is the introduction of new residential. This plan recommends that residential uses be developed immediately adjacent to the village core. Urban Village residential should consist of predominantly mid-density residential, usually 3-4 stories in height.

The availability of land east of the village core is a great opportunity for residential infill. Approximately 25 acres of vacant land exists east of the South Main village core north of East Hattie Street, with additional vacant parcels south of Hattie. The area north of Hattie to Broadway provides the best opportunity for new residential development and open space. Two primary obstacles exist in making this area attractive to new investment: 1) under capacity of infrastructure/utilities and 2) fragmented ownership.

The TCB team presented different development scenarios in this undeveloped area during the second public work session. During those discussions the



The TRE Station presents a great opportunity for transit-oriented development.



Urban village residential will add diversity to the existing housing stock.



Existing conditions at the Interstate 30 gateway need to be improved.

participants agreed the area should gradually be converted to its “highest and best use”. Land banking was considered as an option for assembling and holding vacant tracts for a larger development of new residential/open space use. During the period of holding, and until a market for new residential warranted its development, the land could be designed for youth recreation.

Around the country, youth soccer and ball fields have now become classified as an anchor use for other supportive retail activities. Youth athletic fields are generally inexpensive to develop, while being an effective “placeholder” for future urban residential development. At the same time, the athletic fields would provide a significant early benefit by helping to meet the existing underserved demand for youth athletic fields.

As the demand for new residential units in this area increases, the City should encourage infill along Bryan Avenue and Crawford Street initially, focusing along the Hattie Street corridor and gradually moving northward to East Broadway Street. The area north of Broadway to Vickery Boulevard today is used as industrial.

In the long term, an outdoor entertainment/sports venue could replace the industrial uses between East Broadway and Vickery, as industrial uses would become incompatible over time with a growing residential/mixed-use base. The participants embraced this concept during the second work session as a potential long term option for the area.

3.5 SOUTH MAIN/VICKERY BOULEVARD IMPROVEMENTS

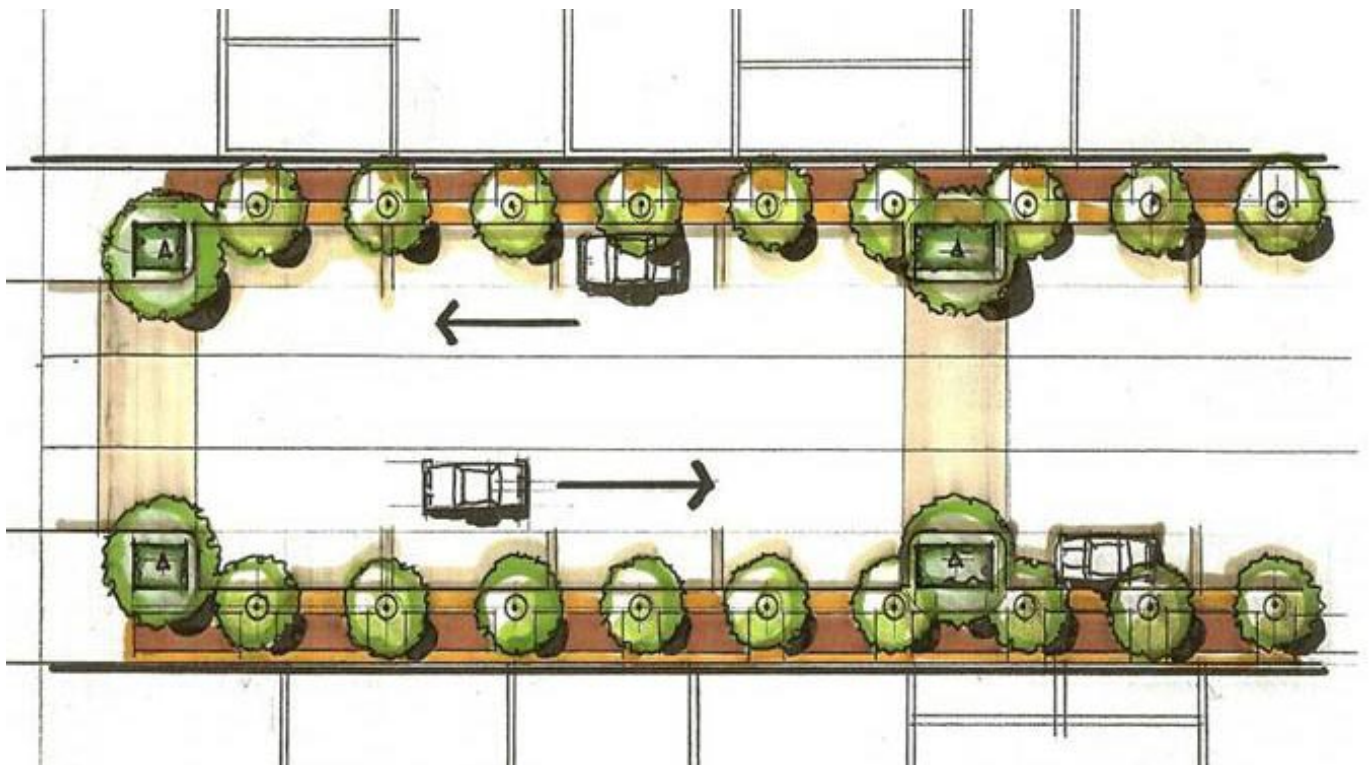
A central part of the urban village master plan is the identification and implementation of infrastructure and transportation improvements. The allocation of federal funding for this effort totals \$740,000. Two options were considered for the expenditure of these dollars at public work sessions: 1) Interstate 30 Gateway or 2) South Main Street.

The consensus of the four break-out groups of citizens from the second work session was to focus the improvement dollars on the South Main/Vickery intersection. Identified improvements to the intersection ranged from pedestrian crosswalks to signage and landscaping. At the third work session however, cost estimates and the desire to address South Main Street in a more comprehensive way resulted in the group wanting to focus the available resources at the South Main/Vickery intersection and Interstate 30 gateway. This gateway has also been selected to receive Public Art resources as a complement to this effort.

The City should focus the allocated resources at the Vickery/Interstate 30 gateway and the South Main/Vickery intersection, taking design options to the public during the next phase of the contract.



Improving the Interstate 30 gateway will enhance the image of the South Main Urban Village.



In public work sessions, participants decided improvements to South Main Street needed further study.

4. IMPLEMENTATION

The South Main Urban Village Master Plan contains both short and long term projects and actions. The plan is intended to be a flexible living document needing update at least every five (5) years. Because the City of Fort Worth has identified multiple urban villages throughout the City, many of the recommendations for implementation can be applied to all urban villages. However, village specific application on policies, plan actions, and catalyst projects vary.

The City of Fort Worth should allocate resources to assist, monitor, coordinate, and market projects within each Urban Village. Pertinent City staff should also act as a liaison between the village and City when determining how public resources will be used on capital projects and other public efforts identified in the plan.

A vibrant urban village plan focuses on three strategies:

1. Policy Tool Box
2. Action Package
3. Catalyst Projects

4.1 POLICY TOOL BOX

The following plan recommendations are based on policy and/or policy support initiatives. Some of the suggestions already exist as City policy while others are new issues applying specifically to South Main Urban Village.

LAND USE FRAMEWORK

The function and mix of uses, buildings, networks, and spaces is critical to establishing a pedestrian-friendly environment and a true urban village experience. The area including South Main Urban Village was recently rezoned with the assistance of the City and Fort Worth South, Inc. The policy and action guidelines contained in this report should be used as incentives for property owners to conform to the more detailed land use plan.

MIXED-USE

The basic T-5 zoning classification is consistent with the provisions of the high intensity mixed-use (MU-2) standards. However, as indicated in the Proposed Land Use Plan on page 17, the T-5 zoning classification should be further refined for a lower and higher density mix of uses in the core areas of the South Main Urban Village. A higher density mixed-use would be more appropriate along the Interstate 30 parcels and South Main Street/Pennsylvania Avenue intersection. This zoning classification encourages desirable combinations of compatible residential, office, retail and selected light industrial uses.

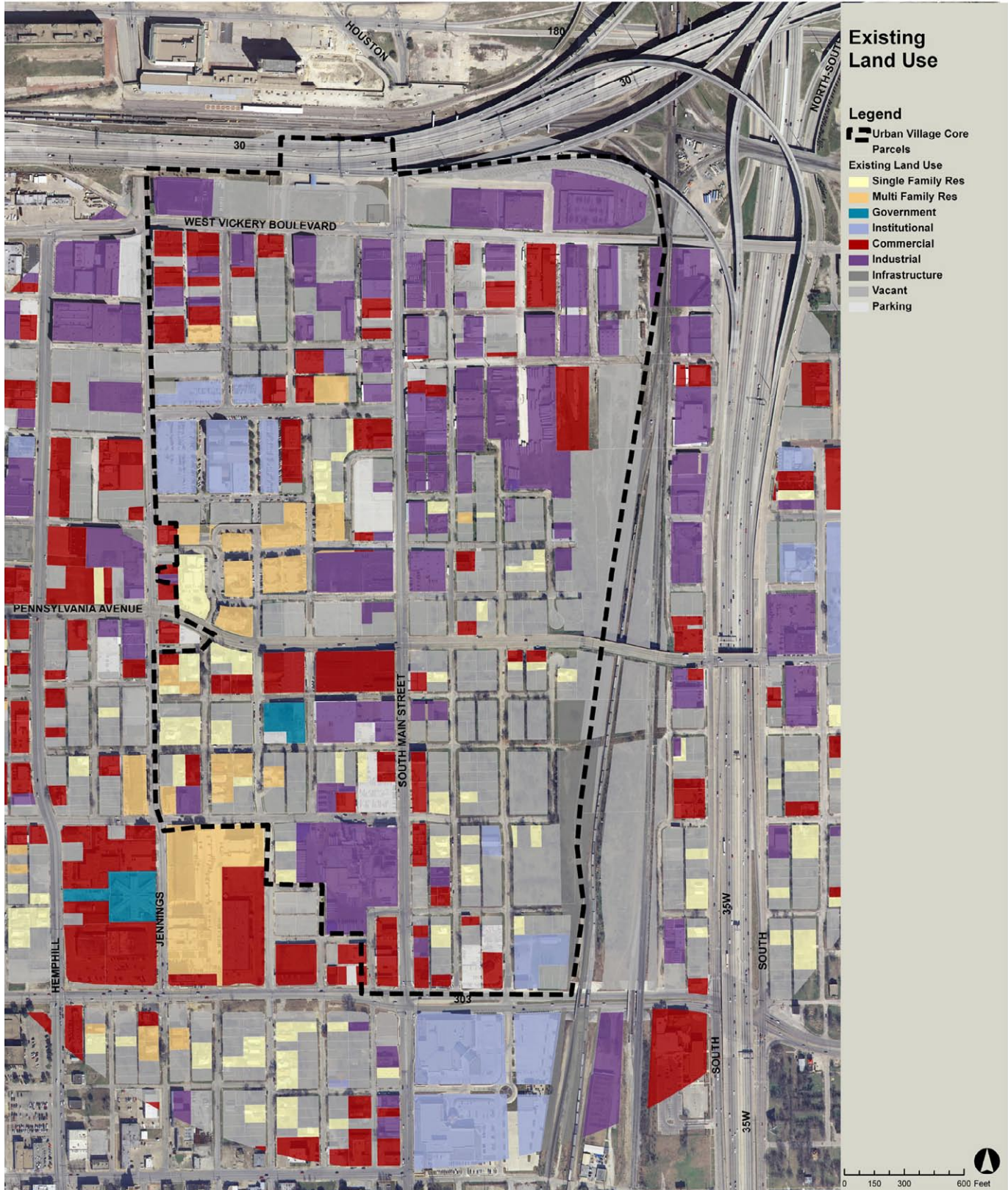
URBAN RESIDENTIAL (UR)

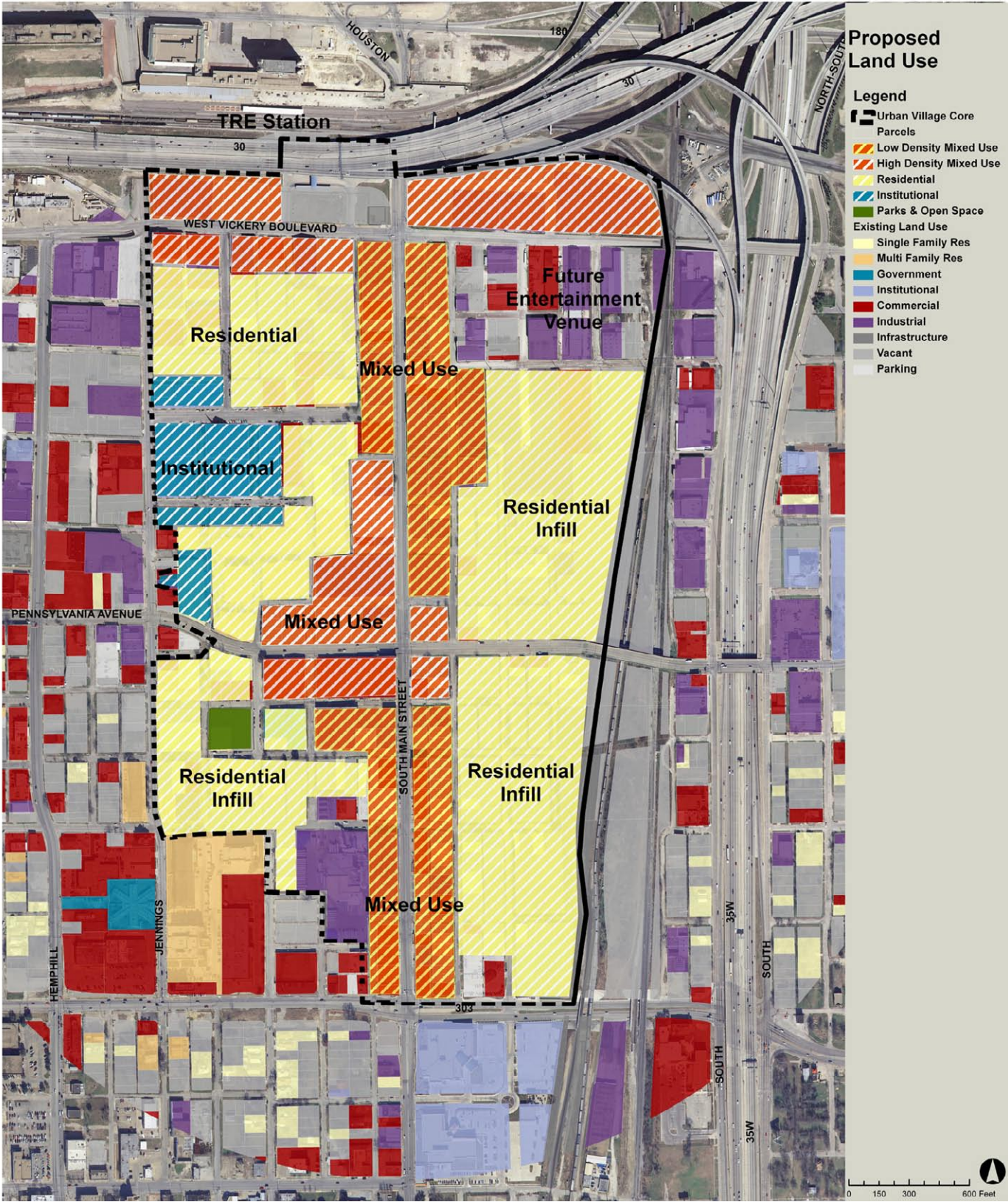
While the policy tool of mixed-use zoning is ideal for the creation of dynamic urban villages, closer attention needs to be paid to surrounding zoning and uses, especially the transition to adjoining neighborhoods. Another concern regarding mixed-use zoning in urban village core areas is the over abundance of retail and office within the greater urban village boundary, thus diluting the strength and viability of the core urban village mixed-use district.

A new zoning classification (urban residential- UR) should be designed to accomplish the goals of higher residential densities while limiting the amount of non-residential uses to only the core areas of the urban village. UR would allow a variety of housing types to exist adjacent to MU, while excluding non-residential uses. The specifications (i.e. density, setbacks, heights, parking, building heights etc.) of the UR would be consistent with MU-1 and the Near Southside Development Standards and Guidelines.

4.2 MARKETING STRATEGY

Today, the urban village of South Main is known by many Fort Worth residents as the area where day labor congregates near the homeless shelters. In order to solidify and promote the South Main Urban Village brand, the plan recommends several strategies both short and long term.







Hosting special events will help market the positive attributes of South Main.



A new organization, the South Main Development Group, should be created to implement this plan.

Some of the marketing initiatives appropriate for South Main include:

- Special events.
- Urban village newsletter.
- Target promotion with downtown & medical district restaurants.
- Briefings to prospective developers/investors.
- Marketing collaboration with other Central Cluster villages.
- Transportation link for downtown and medical patrons.

ORGANIZATION

The South Main Urban Village is fortunate to already be part of Fort Worth South Inc., and the South Main Property Owners Association. To oversee the implementation of the urban village plan, a specific subsidiary (or committee) of Fort Worth South should be created solely for the implementation of the master plan. The South Main Development Group (SMDG) can be supported and administered by Fort Worth South, but organized and managed separately.

The SMDG's central focus is the implementation of the urban village plan. The group should equally represent residents, businesses, and key property owners. This would not be a membership organization; rather, it would represent each of the major organizations and interests. In addition, the SMDG should enlist the services and participation of local financial institutions as members and/or advisors.

STREAMLINED DEVELOPMENT REVIEW PROCESS

As part of the City of Fort Worth's new one-stop shop created last year, development review enhancements and priority for appropriate urban village projects should be provided. A consistent concern from private businesses and developers throughout each of the urban villages is the difficulty and time it takes for permit review and, in some cases, receiving information. This challenge is not unusual for a city the size of Fort Worth. However, cities across the country have moved towards creating a more efficient process for permit applications and review.

While a single location is ideal, having adequate and equipped staff to handle the volume of visitors and knowing how to coordinate and disseminate information is equally important. And while the one-stop system is applied City-wide for all development services, special training and briefings on the importance and goals of the urban village initiative should be given to all City employees involved with the permit review process.

INFRASTRUCTURE ASSISTANCE

In addition to a more efficient permit review process, the other major obstacle within the South Main Urban Village is the inadequate level of infrastructure and utility capacity. The SMDG and the City should design an infrastructure assistance program that would introduce a program for public/private/multi-agency cooperation and partnership for infrastructure upgrades. The costs to upgrade utilities and infrastructure should not be borne solely by the private sector.

An incentive scale should be designed to encourage partnership and land use implementation. And, while urban villages are not subject to transportation impact fees, waivers not already associated with Neighborhood Empowerment Zones should also be made available. Furthermore, partnership with Fort Worth South should allow the SMDG to leverage TIRZ funds to attract other public capital improvement dollars and resources.

necessitate multiple layers of partnership and financing. The South Main Development Group and the City need to coordinate and assign different funding options for each of the components of the master plan.



An infrastructure assistance program will help upgrade critical infrastructure to attract private investment.

URBAN DESIGN FRAMEWORK

With the implementation of new T-5 zoning, the plan recommends that the South Main Urban Village apply the same urban design guidelines used under the Near Southside Development Standards and Guidelines. The plan recommends carrying forward the existing framework of urban design principles which may include: setbacks, building heights, density, open space/landscaping, parking, façades, building materials, entries, and signs.

FUNDING OPTIONS AND INCENTIVES

The City of Fort Worth has made the creation of distinctive, vibrant urban villages a priority. Through its investment in capital improvements and planning documents, the City continues to lead the effort in preserving and enhancing these unique urban places. However, in order to sustain a long term successful vision and plan, other financing partners must be equally engaged.

As part of the implementation section of this plan, various components and catalyst projects will



Urban design guidelines will help ensure new development is of a desirable character appropriate for South Main.

Some of the funding options critical to the long term implementation of the plan include:

- Financial institutions with target programs and investment within the South Main Urban Village.
- Private investment and developers.
- City's annual budget.
- Future bond issuances.
- Creation of a Public Improvement District.
- Medical District Tax Increment Financing.
- Other incentives through neighborhood empowerment zones.

4.3 ACTION PACKAGE

South Main Urban Village is fortunate to be near the recent Lancaster improvements and new downtown loft development along Interstate 30. This momentum of private investment can be heightened by other actions taken immediately by property owners, businesses, residents, and the City. The following action items make up a comprehensive package of short term initiatives that can maintain the positive momentum.

ACTION-FAÇADE PROGRAM

In most examples across the country, urban revitalization happens one building at a time; one block at a time; and one village at a time. By focusing resources on individual buildings, façade improvements can create a snowball effect of new investment and a resulting increase in property values.

Older commercial corridors throughout the United States have particular challenges to overcome, including building neglect, vacancy, and crime. One of the biggest challenges is maintaining the appearance of older buildings, such as those located south of downtown.

Many owners of older commercial buildings would invest in their property if there were more certainty that such investment would yield greater value. Some



Façade improvements can go a long way towards increasing value.

owners fear higher property taxes shortly after out-of-pocket investment is made to a building. Low interest financing is also difficult to obtain, especially in areas of the City where financial institutions may be leery to invest.

To help offset the challenges described in the above paragraphs, the City should consider creating a low interest rate façade improvement program. The City of Fort Worth should create a 12 month “pilot” matching loan program to support building façade improvements. The suggested program could kick-off as early as 2008, after a three month public relations and outreach media blitz. The program should include the following:

- City-wide Urban Village Fund of \$500,000.
- Offered on a first come, first served basis.
- Quarterly accountability and review.
- Low interest rate loan.
- Maximum loan amount of \$50,000/building.
- Require a dollar for dollar private sector match.
- Five-year property tax freeze on new improvements.



Land partnerships will help achieve plan recommendations, including the creation of more green space.

ACTION-LAND PARTNERSHIP

One of the most difficult challenges in the creation of an urban village within an older, commercial district is the ability to implement critical elements of the overall plan amidst the existing development pattern. In the case of South Main, several public projects and the creation of an anchor site are considered critical to the overall plan's success.

The public projects identified within the South Main plan include common area parking, shared commercial service areas, and park/open space around new residential areas. All of these public elements are found within suburban town center developments; the same developments which lured the base market away from the locations of the current urban villages. To be competitive with suburban or new downtown destinations/attractions, the South Main Urban Village must provide adequate on-street and surface lot parking, as well as identify common service areas and public spaces, rather than each building providing their own. The common park and public spaces should provide the needed areas for pedestrians to intermingle between destination stops.

It is rare for a public element within an overall development to include a private purpose. However, in the case of South Main, a private component lacking from recent private investment is the stability of a “Main

Street” anchor tenant. The plan recommends that the City designate the Fort Worth School District Site as an anchor site for the second phase of the South Main Street core revitalization effort.

As described throughout the urban village plan, locations have been identified for common area parking and service areas, park and public spaces, and an anchor site. These locations are priority parcels as part of a land partnership strategy. These parcels should be part of a short and long term strategy of land acquisition by both private and public partners.

The City, South Main Development Group (SMDG), and affected property owners should mutually strategize on how to realize the vision for South Main. The following actions are recommended:

- SMDG facilitate parking agreements with adjacent property owners for joint-use parking and parking area enhancements.
- SMDG coordinate and facilitate with private property owners in the area between East Hattie and Broadway Avenue an area for park/open space and common service areas within the village core, along with an agreement on use.
- SMDG begin discussions with Fort Worth Independent School District, or a subsequent owner, to facilitate the anchor site development. SMDG, through Fort Worth South's tax-exempt status, can provide tax breaks to private property owners in the sale of the property.

ACTION-NO VACANCY

As a priority area for private investment, the South Main Street core area, between Vickery Boulevard and Annie Street, requires a comprehensive approach to attracting a mix of office, retail, and housing opportunities. Operation No Vacancy is recommended as a holistic approach to decrease building vacancy using market information and existing building footprints.



Market information and existing buildings should be used to attract and retain new uses.

Today, the South Main Street core area has a total of 305,000 square feet of space. Approximately 254,000 square feet of total space is at street level. Approximately 55% (160,000 square feet) of total core area building space is currently vacant. Based on preliminary market analysis, the following market demand is projected for the urban village for a 10-year period:

- 150 to 200 condo/townhome/ loft units.
- 250 to 300 rental housing units.
- 150,000 to 200,000 square feet of office.
- 50,000 to 100,000 square feet of retail.
- 100 to 150 room boutique hotel.

Thus, a primary mission of the South Main Development Group is to increase the occupancy rate of existing building space. Based on market information, a reasonable goal of 80% occupancy by the year 2015 is achievable. The SMDG should coordinate with the City in a joint marketing effort intended to fill existing vacant space. A comprehensive inventory analysis of existing buildings needs to be conducted for an accurate understanding of available space.

From an inventory analysis, the SMDG and City can identify the type and size of tenants to target for marketing efforts. A professional marketing brochure should be part of the coordinated marketing effort to attract an appropriate tenant mix. Stakeholders can

use information from the market analysis as part of their strategic effort to attract the target tenant mix.

Demographic data can also be helpful while marketing the area to private interests. In 2007, the trade area population stood at 37,000 persons with 55% renting, a median age of 34, nearly 30% blue collar, a median household income of \$32,196, and a population of 33% Hispanics and 30% African-Americans.

ACTION-MOBILITY & ACCESSIBILITY

The primary physical characteristic supporting a vibrant mixed-use urban village is a pedestrian-friendly built environment. As part of the overall village plan, pedestrian amenities must be enhanced throughout the village area. Sidewalks, lighting, and signage are all basic examples of pedestrian elements necessary for good mobility and accessibility. The City should pursue funding through various local, state, federal, and other sources for the implementation of these improvements.

The priority location for mobility and accessibility improvements is South Main Street, first in the core district, and then to continue south to Rosedale Street. South Main improvements to accommodate wider sidewalks and updated utilities should be the first phase, leading to a more village-wide improvement effort. Second phase improvements should concentrate in the areas identified as future Transit-Oriented Development and School District Anchor Site. The last phase should include the remaining areas of the village.

The SMDG and the City should develop a comprehensive strategy as part of this phased mobility and accessibility effort. Additional pedestrian improvements, separate from those along South Main Street, include the following:

- Village-wide sidewalk improvements.
- Village-wide pedestrian crosswalks.
- Consistent and appropriate landscaping.
- South Main unique street/pedestrian lighting.
- Uniform signage (consistent with Near Southside Development Standards and Guidelines).

ACTION-SAFE ZONE

Public safety and the perception of crime is a universal challenge for all urban villages in Fort Worth. The plan envisions a comprehensive approach and proactive solutions for reducing crime and making the urban village feel safer. The coordination of initiatives and programs aimed at public safety needs buy-in by elected leaders, the Fort Worth Police Department, and the community.

The plan recommends the implementation of a Safe Zone strategy for South Main Urban Village. This strategy would bring together under one umbrella programs already in use as well as shared resources between various urban villages.

IMPLEMENTATION OF SAFESCAPE PRINCIPLES

SafeScape is a holistic approach for responding to the issues of crime and personal safety that plague many neighborhoods, business districts, and communities. The application of SafeScape requires only a basic understanding of how to use the principles to determine why people may not feel safe in a given situation and then how to respond to improve the situation. There are seven SafeScape Principles:

HUMAN FACTOR PRINCIPLES

1. Information and Orientation: We feel unsafe when we don't know where we are and/or where we are going.
2. Interaction and Socialization: We feel unsafe when we are alone and there are no other people with whom we can interact.
3. Ownership and Stewardship: We feel unsafe when the physical environment is not properly cared for and not maintained.
4. Seeing and Being Seen: We feel unsafe when we can't see other people and they can't see us.

IMPLEMENTATION PRINCIPLES

5. Land Use and Design: Encourages safety and community-building through proper design of the physical environment.



Mounted police patrol can help increase public safety and reduce crime.

6. Activity and Programming: Facilitates safety and community-building by bringing people together in the physical environment.
7. Management and Maintenance: Sustains safety and community-building through the long-term commitment to proper care of the physical environment.

Source: www.downtowndevelopment.com

SURVEILLANCE CAMERA PROJECT(S)

The Fort Worth Police Department is currently evaluating its capacity to implement surveillance camera projects in appropriate areas of the City. Surveillance cameras could be used in the urban village to increase the visibility of the Police Department, as well as to help apprehend persons that commit crimes in the area. The Police Department has yet to determine the particular areas of the City to implement a camera project.

OPENING OF A POLICE STOREFRONT

The Fort Worth Police Department operates a total of 20 Police Storefronts. These serve as small office areas provided to the City at no or low cost by a landlord and are sometimes utilized by Neighborhood Police Officers (NPOs) and beat officers to complete paperwork and to assist the public with community problems. Currently, there is a storefront at the Six Points Urban Village, as well as adjacent to the Historic

Marine Urban Village. However, it is possible that a storefront would be beneficial to the South Main Urban Village and could represent a positive public improvement to support community policing.

SHARED BIKE PATROL

The Fort Worth Police Department currently has bike officers that patrol the central business district (CBD). The Bike Unit was re-established in 1989 as a tactical response to apartment crimes in the Woodhaven area. The mobility of the bike and resulting close contact officers had with citizens added great value to our Community Policing efforts. In 1991, the Fort Worth Police Department recognized the need to increase the police presence and mobility in the Central Business District. The Foot Patrol Unit evolved into the City's current Downtown Bike Patrol Unit.

Today, the unit consists of 18 full-time bike officers working on three shifts. These highly trained and motivated officers respond to calls for service, enforce traffic violations, and conduct surveillance to deter criminal activity. Each officer is issued a specially fitted bicycle, wears a distinctive bike uniform, and receives 40 hours of training on the tactical uses of bikes in police work. The versatility of the bicycle allows officers to easily navigate the congested streets within the business district. Bike Officers interact with citizens to provide a sense of security, a source of information, and a visible presence.

The Urban Village Program could benefit from the addition of bike patrols, particularly within the Central Cluster (Six Points, South Main, and Historic Marine). To implement bike patrols in these areas, the Neighborhood Police Officers (NPO) have the option of becoming bike-certified and requesting a donated bike from the Bike Support Group. This would allow for bike patrols to occur with minimal resources.

Other public safety strategies possible for each of the Central Cluster villages include:

- Central Cluster crime analysis using Geographic Information System data identifying hot spots and trends.
- Patrol car assignments based on GIS hotspots.

- Joint “village night out” events in collaboration with other Central Cluster villages.

ACTION-URBAN VILLAGE SWEEP

In addition to attracting new investment into an urban village, code enforcement and upkeep of existing structures is equally important. The “nuts and bolts” of revitalization includes a comprehensive approach to establishing trust and a strong bond between community and City. Rather than only focusing on code enforcement violations within the village, the plan recommends a more comprehensive “good-will” approach to clean-up: Urban Village Sweep.

The village sweep is envisioned as a month-long comprehensive bundle of City services in addition to a year-long follow up of services. A command post would be established where residents can voice their concerns and inquire about all City services. Through community meetings, residents are empowered to make requests for services and report concerns in an effort to customize services to each individual neighborhood's needs and to ensure efficiency in their delivery.

In addition, door-to-door visits and property checks could be made. Some of the services included in this mobilization effort might include:

- Code enforcement.
- Housing rehabilitation programs.
- Energy savings programs.
- Brush and trash pick up.
- Crime “hot spot” surveillance.
- Health department mobilization.
- Animal Care services.
- Employment opportunities.
- Street sweepers.
- Graffiti clean up.

APPENDIX - BEST PRACTICE EXAMPLES

BEST PRACTICE EXAMPLES

- Southtown - San Antonio, Texas
- Pearl Distric - Portland, Oregon
- Frisco Main Street & Sports Complex

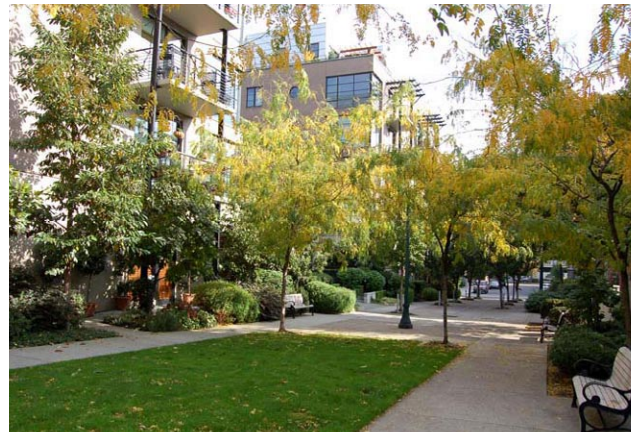
SOUTHTOWN

- Human scale urban village.
- Proximity to CBD.
- New Development (500 apartment units & ground-level retail space).
- Historic fabric.
- Converted warehouses and industrial buildings.
- Mixed-use zoning.



PEARL DISTRICT

- Access to multi-modal transportation.
- Proximity to CBD.
- Former Industrial Area.
- An urban success story:
 - Over 3500 condos and apartments.
 - More than 50 restaurants.
 - Existing Whole Foods, Super Safeway grocer store planned.



FRISCO SQUARE SPORTS CENTER

- Main Street design.
- Pedestrian friendly.
- Mixed-use.
- Health and recreation emphasis.
- Entertainment destination with sports theme.
- Youth soccer anchor.

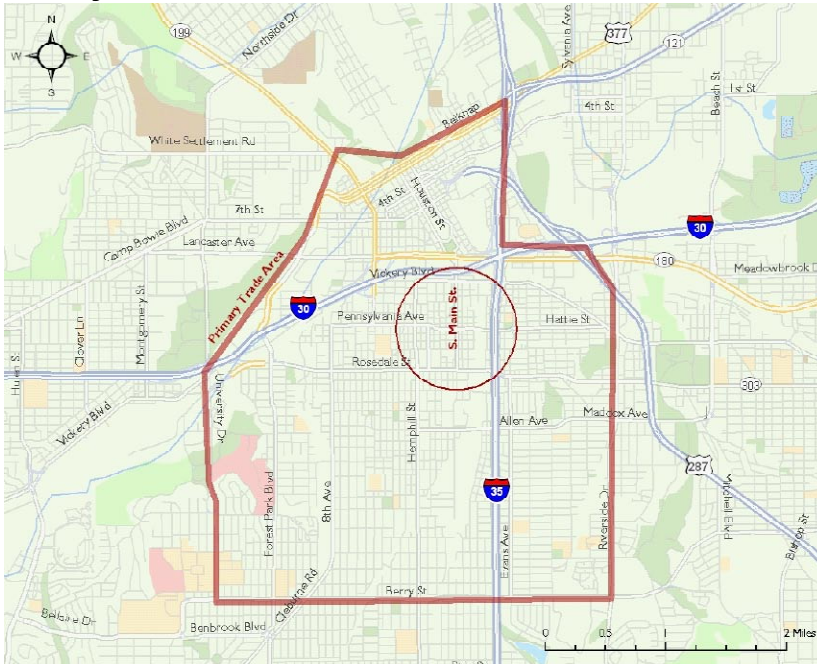


APPENDIX - MARKET AND DEMOGRAPHIC DATA

Market Overview

South Main Village Fort Worth, Texas

Primary Trade Area



Sources: North Central Texas Council of Governments; U.S. Census; ESRI (census-based data); State of Texas Labor Market Information; Costar Inc. (commercial real estate data); Leland Consulting Group

Retail emphasis should be dining, specialty shops, live/work shops and smaller-scale entertainment

Office emphasis should be on professional, design, educational, medical/dental/alternative health

Residential opportunity is strongest for apartments, attached ownership housing in a vertical mixed-use setting

Office Market

	Trade Area	City of FW
Total Rentable s.f.	14.8 M	30.0 M
Pct. Vacant (Q2-2007)	7.3%	8.5%
Avg. Rent/sf	\$20.93	\$17.94
Est. Ann. Job Growth Rate	1.1%	1.5%
Est. 10-yr. Office Demand	2.4 M sf	
S. Main Study Area Demand Share (10 yr)	180,000 sf	

Retail Market

	Trade Area	City of FW
Total Rentable s.f.	1.74 M	32.3 M
Pct. Vacant (Q2-2007)	5.4%	9.4%
Avg. Rent/sf	\$15.11	\$13.49
Est. Ann. Hhld. Growth Rate	1.5%	1.6%
Est. 10-yr. Retail Demand	350,000 sf	
S. Main Demand Share (10 yr)	85,000 sf	

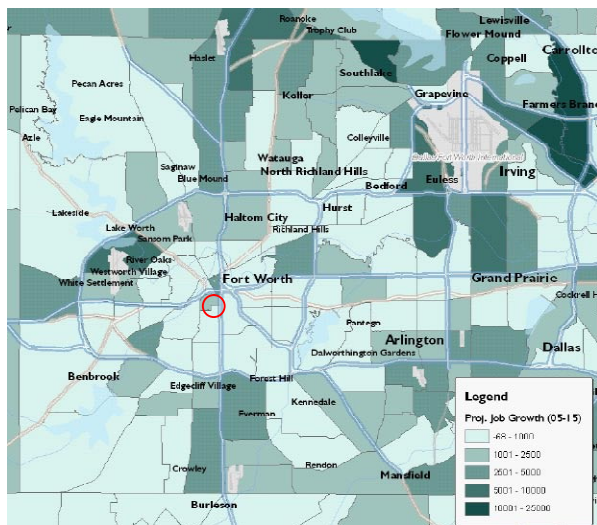
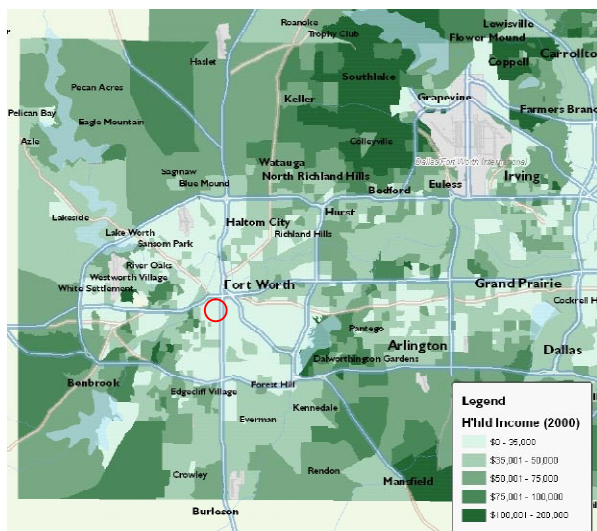
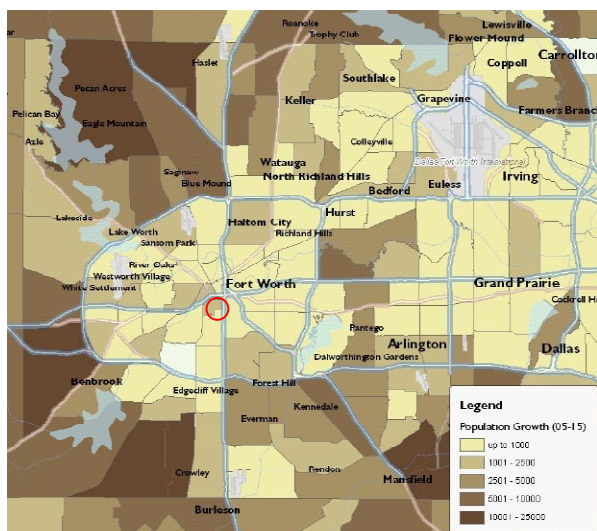
Residential Market

	Trade Area	City of FW
Existing Households (2007)	12,968	250,583
Est. Ann. Hhld. Growth Rate	1.5%	1.6%
Est. 10-yr. Total Unit Demand	2,186	
S. Main Study Area Condo-TH-Loft Demand Share (10 yr)	175 units	
S. Main Study Area Apartment Demand Share (10 yr)	260 units	

Market Strengths

- Enhanced access to downtown with improvements to Lancaster.
- "Good bones" in many historic structures should respond well to streetscape enhancements, with opportunity for vertical mixed-use.
- Strong freeway traffic volume and potential for excellent freeway visibility.
- Proximity to multiple medical campuses, as well as a vibrant downtown.
- Redevelopment activity at Texas & Pacific, Magnolia Green, Homes @ Parker Commons, St. Joseph's Gardens, etc. reflects Fort Worth South's potential.
- But, must overcome extensive commercial vacancies and poor reputation of immediate neighborhood.**

Trade Area Profile

South Main Village
Fort Worth, Texas

	Trade Area	City of Fort Worth
2007 Population	36,908	686,850
2007 Households	12,968	250,583
Annual Household Growth Rate (Projected through 2030)	1.5%	1.6%
Average Household Size (2007)	2.54	2.69
Pct. Non-family Households (2007)	43.4%	34.6%
Pct. Renters (2007)	55.3%	43.0%
Pct. Age 65+	12.5%	9.3%
Pct. Age 0-14	19.1%	23.2%
Median Age	34.5	32.1
Pct. With Bachelors Degree	19.6%	22.3%
Pct. Blue Collar (Age 25+)	28.9%	26.0%
Median Household Income	\$32,196	\$47,229
Per Capita Income	\$19,670	\$23,609
Pct. With Income Below \$25,000	47.3%	32.3%
Pct. With Income Over \$100,000	11.9%	16.5%
Pct. Hispanic	32.7%	29.8%
Pct. Black/African-American	29.8%	20.1%
Top Tapestry™ Lifestyle/ Psychographic Segments	Modest Income Homes	South-western Families
	Metro Renters	Milk and Cookies
	South-western Families	Young and Restless

APPENDIX - HISTORIC INVENTORY DATA

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



KOMATSU
ARCHITECTURE

Address:	104 S. Main Street		
Building Name:	Safeway Store No. 332		
Historic Designations:	None, (Tarrant County Historic Resources Survey, Phase III)		
Current Usage:	Commercial	Year Constructed:	1940
Building Construction:	Historic, one-story reinforced concrete		
Notes:	Art Moderne style		

Recommendations:

Excellent opportunity for restoration and continued use as commercial or retail property. Seen as contributing to overall historic character of the Urban Village.

Further recommended rehabilitation:

1. Restore façade to historic or adaptive use capacity
2. Redevelop property for enhanced parking, landscaping in conjunction and complementary to the overall objectives of the Village.
3. Develop signage or other means of informing and educating public and visitors to the Village regarding the historic importance of the structure as part of overall Village program. This to be part of overall cultural resources identification system.

Photograph:



Photograph Description:

View from Main Street, southeast

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



KOMATSU
ARCHITECTURE

Address: 219 South Main

Building Name: Hodge Chemical Company (earliest known occupant)

Historic Designations: None, possibly eligible NRHP, (Tarrant County Historic Resources Survey, Phase III)

Current Usage: Commercial **Year Constructed:** c. 1906

Building Construction: Historic, three-story masonry

Notes: Original usage was commercial

Recommendations:

Retain and Maintain as excellent example of load bearing brick commercial structure.

Recommended restoration:

1. Strip paint from brick; restore brick facades.
2. Install awnings in accordance with historic photograph
3. Develop signage or other means of educating public/visitors regarding historic importance of the structure as part of overall Village program. This to be part of overall cultural resources identification system.

Village Context: maintain structure as continued excellent example of historic fabric contributing to the overall character of the South Main area

Photograph:



Photograph Description:

View from Main Street, south / southwest

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



KOMATSU
ARCHITECTURE

Address:	Property Address: 1700 to 1800 Blocks of Main Street		
Building Name:	Historic Main Street Underpasses		
Historic Designations:	Eligible NRHP, (Tarrant County Historic Resources Survey, Phase III)		
Current Usage:	Underpass	Year Constructed:	1931-1932
Building Construction:	Historic, reinforced concrete		
Notes:	One of four distinct 1930s era concrete underpass/viaducts constructed by the City and the Texas and Pacific Railroad to move automobile traffic under the railroad ROW.		

Recommendations:

- Recommend full retention of all the underpasses in this area
1. Recommend restoration of underpasses,
 2. Recommend installation of aesthetic and security lighting systems.
 3. Recommend landscaping enhancements adjacent to screen highway overpass elements at sides as viewed from southerly direction.
 4. Recommend painting of elements of the overpass to blend with overall earthtones of overpass system.
 5. Develop cultural resources identification system for entire South Main Urban Village; provide identification and visitor information system for underpass systems.

Photograph:



Photograph Description:

View from Industrial Street looking west

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



Address: 201 E. Daggett Avenue

Building Name: Salerno Building (Current: unknown)

Historic Designations: None (Tarrant County Historic Resources Survey, Phase II)

Current Usage: Unknown **Year Constructed:** C. 1909

Building Construction: Two-story load bearing brick masonry

Notes: Building originally housed the saloon of Jacob Salerno

Recommendations:

Retain and Maintain as excellent example of load bearing brick commercial structure.

Recommended restoration:

1. Strip paint from brick; restore brick facades.
2. Develop signage or other means of educating public/visitors regarding historic importance of the structure as part of overall Village program. This to be part of overall cultural resources identification system.

Village Context: maintain structure as continued excellent example of historic fabric contributing to the overall character of the South Main area

Photograph:

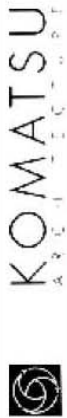


Photograph Description:

View from Daggett Ave. looking northeast

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



Address:	211 Bryan Avenue
Building Name:	Brotherhood of Locomotive Firemen and Enginemen (Current: unknown)
Historic Designations:	Eligible NR/Rail (NR) (Tarrant County Historic Resources Survey, Phase III)
Current Usage:	Unknown
Building Construction:	Two-story load bearing brick masonry
Notes:	Year Constructed: C. 1910 Site originally occupied by Trinity Lodge of the BLR&E in 1885
Recommendations:	Retain and Maintain as excellent example of load bearing brick commercial structure. Recommended restoration: 1. Restore brick facades in accordance with historic data. 2. Develop signage or other means of educating public/visitors regarding historic importance of the structure as part of overall Village program. This to be part of overall cultural resources identification system. Village Context: maintain structure as continued excellent example of historic fabric contributing to the overall character of the South Main area

Photograph:



Photograph Description:

View from Bryan Ave. looking southeast

South Main Urban Village, Fort Worth, Texas

Existing Building Survey



KOMATSU
A Z C I T C P F

Address:

212 South Main Street

Building Name:

Unknown (Current: ABC Flag Company)

Historic Designations:

Unknown

Current Usage:

Commercial

Year Constructed: Unknown

Building Construction:

Two-story load bearing brick masonry

Notes:

Recommendations:

Retain and Maintain as excellent example of load bearing brick commercial structure.

Recommended restoration:

1. Restore brick facades in accordance with historic data.
2. Develop signage or other means of educating public/visitors regarding historic importance of the structure as part of overall Village program. This to be part of overall cultural resources identification system.

Village Context: maintain structure as continued excellent example of historic fabric contributing to the overall character of the South Main area. Maintain as current usage.

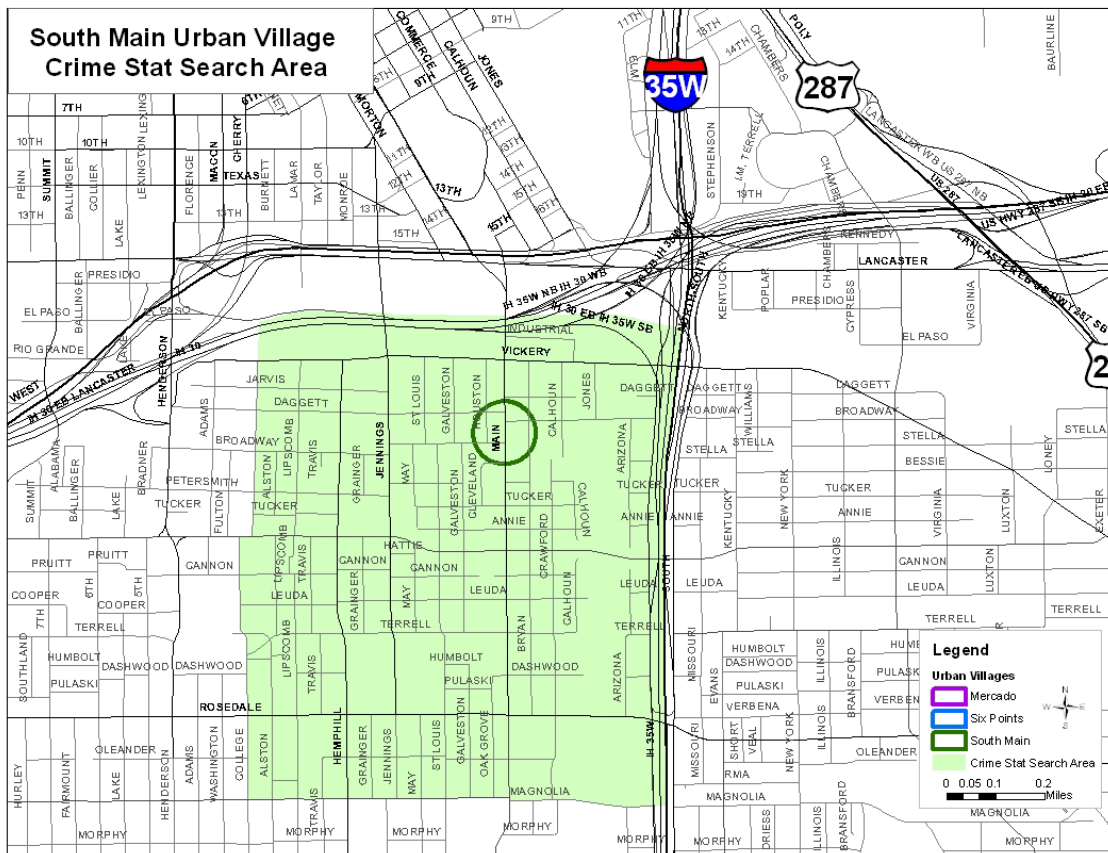
Photograph:



Photograph Description:

View from South Main Street looking northwest

APPENDIX - CRIME TRENDS



Since 1991, Part I Crime has decreased by 70%
 Since 1995, Part I Crime has decreased by 26%
 Since 2000, Part I Crime has decreased by 10%

Comparing Year-to-date 2007 to the same time in 2006
 367 Part I Crimes in the area for Jan – Sept 2007
 317 Part I Crimes in the area for Jan – Sept 2006

APPENDIX - NEAR SOUTHSIDE DEVELOPMENT STANDARDS & GUIDELINES



NEAR SOUTHSIDE Development Standards and Guidelines



Prepared by Fort Worth South, Inc.
in cooperation with the City of Fort Worth

October 2007

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TABLE OF CONTENTS

Section 1. Introduction	
A. Background	3
B. Promoting Revitalization with Form-Based Standards	3
Section 2. General Development Principles	
A. Intent	5
B. General Development Principles	5
Section 3. Administration	
A. Intent	7
B. Relationship to Other Regulatory Documents	7
C. Definition of “Standards” and “Guidelines”	7
D. Development Review Process	7-8
E. Near Southside District Boundary	9
Section 4. Regulating Plan	
A. Intent and Principles	11
• How to Use the Regulating Plan	11
• The Transect Classification System	12
B. Maps of Development Zones and Historic Properties	13-16
C. Circulation Network and Thoroughfare Classification	17
• Maps	18-21
Section 5. Development Standards and Guidelines	
A. Intent and Principles	23-24
B. Streets and Other Public Spaces	25-34
• Streets	25-30
• Roadsides	31-33
• Public Parks and Plazas	34
• Public Art	34
C. Building Location and Orientation	35-36
D. Building Height	37
E. Parking and Driveways	38
F. Architectural Standards	39-40
G. On-Site Landscaping	41
• Canopy Tree List	42
Section 6. Permitted Land Uses	
A. Mix of Use Requirement for Large Developments	43
B. Residential Requirement for “N” Neighborhood Zones	44
C. Permitted Land Uses	44-52
Section 7. Appendix	
A. Sign Guidelines (excerpted from Downtown Sign Standards)	
B. Zoning Ordinance Page for Near Southside “NS” District	

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1.A. Background

The Near Southside is located just south of Downtown Fort Worth and is the city's second largest employment center, with over 30,000 workers. Approximately 18,000 of these workers are employed in the health care industry. Although commonly recognized today as the "Medical District," the area has historically been a mixed-use district with diverse neighborhoods and eclectic destinations. The area's remaining historic buildings – with their architectural diversity, human-scale design, and wide range of locally-owned businesses – reflect this storied past.

Fort Worth South, Inc. (FWSI), a non-profit redevelopment corporation, has worked since 1996 with the City of Fort Worth and other community groups to promote the area's revitalization in a way that builds on this history and capitalizes on the district's eclectic, mixed-use, human-scale character to create a competitive advantage in attracting new residents and businesses. Urban design (i.e., the design of buildings, streets, and other public spaces, and the interrelationships among those elements) will continue to be a major factor in determining the district's economic, social, and cultural sustainability. Promoting the sustainability of neighborhoods and commercial districts is a primary goal for the City of Fort Worth.

1.B. Promoting Revitalization with Form-Based Development Standards

As reflected in the City's Comprehensive Plan (which includes the FWSI strategic plan by reference), FWSI and the City have long recognized the importance of urban design in realizing revitalization goals for the Near Southside. As in the past, the district's future character will largely be determined by the individual design decisions made by hundreds of property owners, businesses, and residents over the following decades. The City's zoning regulations and other development standards play perhaps the most influential role in guiding these decisions, and, as a result, in shaping the Near Southside's character, quality of life, and sustainability.

For decades, zoning regulations, street design requirements, and other development standards have worked against urban design and redevelopment goals for the Near Southside and other central city districts. In response, the City adopted new form-based "MU" mixed-use zoning classifications, and, in certain central city areas such as Downtown and Trinity Uptown, created new urban design districts with tailored development standards and guidelines that promote development that is consistent with the contextual design goals for those respective areas.

FWSI has worked with property owners to promote desirable redevelopment in two ways: first, through rezonings to the "MU" classifications; and second, through the application of informal design guidelines. Although the effects of these past efforts have been significant, a comprehensive update of the district's development standards – through the creation of a new Near Southside urban design district overseen by the Urban Design Commission – is necessary to promote the district's continued revitalization, for several reasons:

- The patchwork of single-use industrial, commercial, and residential zoning districts did not allow or promote a **mix of residential, retail, office, and institutional uses** as envisioned in the City's Comprehensive Plan, within which the Near Southside is designated as a "mixed-use growth center."
- Outdated development standards did not promote the type of **pedestrian-oriented urban design** that characterizes the Near Southside's most distinctive and memorable areas and serves as a competitive advantage for the district.
- The patchwork of zoning districts threatened redevelopment momentum by failing to create a **predictable investment environment** in which property owners could confidently invest in redevelopment projects that conformed to the district's vision and be reasonably assured that zoning standards would require compatible development on adjacent properties.
- The wide range of conditions within the district – from lower density residential neighborhoods to large institutional campuses – required a **more tailored approach than simply applying all of the "MU" standards** throughout the area.
- Design excellence, administrative flexibility, and timely approval of conforming projects would best be promoted through a **development review process overseen by a well-qualified design review board**, as demonstrated by the Downtown Urban Design District process.

This illustrated document uses photographs and other images in an effort to create a user-friendly manual for use by professional developers as well as non-experts. The standards and guidelines contained are largely based on the form-based standards of the "MU" classifications. Form-based standards differ from conventional zoning standards in their emphasis on shaping the form of buildings and public spaces rather than the segregation of different land uses.

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2.A. Intent

The Near Southside will become a vibrant, mixed-use, urban district if certain general development principles shape its redevelopment. The following principles serve as the basis for the standards and guidelines contained in this document. Development projects are evaluated with respect to the principles, in addition to the pertinent standards and guidelines.

2.B. General Development Principles

1. **Promote a pedestrian-oriented urban form.**

In contrast to conventional zoning standards that place primary emphasis on the regulation of land uses, the Near Southside development standards and guidelines focus on promoting a walkable, urban form of development, consistent with the district's historic urban character. The focus on form promotes buildings that conform to tested urban design principles, and that adapt to changing conditions over time.

2. **Maximize connectivity and access.**

The Near Southside's successful revitalization requires a truly multimodal circulation network in which residents, workers, and visitors may conveniently walk, drive, bike, or ride public transportation to destinations within and outside of the district. Development standards and guidelines are intended to promote walkable blocks and street designs that balance these transportation modes, and also ensure accessibility for all residents and visitors, including those with disabilities.

3. **Require excellence in the design of the public realm (building on Fort Worth's history of civic art) and of buildings that front public spaces.**

The most successful and memorable urban environments are those in which walking down the street is appealing. Streets, plazas, parks, and other public spaces should be comfortable and inviting, and buildings fronting those spaces should be active and visually interesting at the pedestrian level.

4. **Promote the preservation and creation of distinctive neighborhoods that provide diverse urban housing options.**

The vision for the Near Southside places high priority on the creation of mixed-use, mixed-income neighborhoods that include townhouses, apartments, condominiums, and supporting neighborhood-scale businesses. Providing a large number of units and options for various household types and income levels is critical to the district's success.

5. **Support existing Near Southside businesses.**

The Near Southside development standards and guidelines should support the success of existing businesses.

6. **Encourage adaptive reuse and support the preservation of historically significant buildings.**

The Near Southside includes historically significant buildings that should be incorporated into new development projects. The development standards and guidelines are intended to work in conjunction with the City's Historic Preservation Ordinance to encourage preservation and adaptive reuse of significant buildings.

7. **Encourage creativity, architectural diversity, and exceptional design.**

The Near Southside development standards and guidelines promote high quality design, and the development review process promotes flexibility. Standards and guidelines, as well as the development review process, are intended to support creativity and exceptional design while discouraging uniformity.

8. **Promote sustainable development that minimizes negative impacts on natural resources.**

Creating a walkable, mixed-use, high-density, central city district supports sustainable development by providing an alternative to low-density development in peripheral areas. In accordance with sustainable development principles, the Near Southside's buildings, transportation systems, and public spaces should be designed to minimize negative impacts on air and water quality and promote innovation in environmental design.

9. **Encourage the integration of public art into public and private development.**

Public art should be integrated into architecture, streetscapes, and public spaces. Public art will enhance the built environment and contribute to the area's success in attracting new residents and businesses.

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3.A. Intent

The development standards and guidelines included in this document are intended to be clear, concise, and user-friendly. The development review process is intended to be predictable and flexible, and to facilitate the timely approval of conforming projects.

3.B. Relationship to Other Regulatory Documents

The Near Southside (NS) Development Standards and Guidelines document is intended to minimize references to other sections of the City's Zoning and Subdivision ordinances. Developers are responsible, however, for reviewing and complying with all pertinent zoning and subdivision standards, including those not directly referenced in this document.

There are three notable references to other sections of the Zoning Ordinance: a) the Historic Preservation Ordinance (Chapter 4, Article 5), which includes the procedures and requirements related to the preservation and reuse of historically significant buildings; b) Off-Street Parking and Loading (Chapter 6, Article 2), and c) the Sign Ordinance (Chapter 6, Article 4) and Downtown Sign Standards. The sign ordinance includes basic dimensional standards, and the Downtown standards are applied as design guidelines within the Near Southside district (see Appendix A).

All development must comply with all federal, state, county, or city regulations. Historically designated properties must comply with requirements associated with those designations (e.g., Fairmount Historic District properties must comply with that district's design guidelines as well as processes required by the Historic and Cultural Landmarks Commission).

3.C. Definition of "Standards" and "Guidelines"

Development Standards are objective, measurable regulations, often illustrated through diagrams and sketches, with which all projects must comply. Unless noted as a guideline, all provisions in this document are development standards. If a project of exceptional design is clearly consistent with the General Development Principles but does not conform to a certain development standard, the Urban Design Commission (UDC) may approve a Certificate of Appropriateness (COA) that cites the project's compliance with those principles. (For projects not conforming to the height and setback regulations listed in the Zoning Ordinance as shown in Section 7, Appendix B, a variance approved by the Board of Adjustment is also required; see Development Review section below.)

Development Guidelines are more subjective statements through which the City proposes additional design strategies. The guidelines should be suitable for most projects, and developers should endeavor to ensure that guidelines are followed to the extent possible. City staff and the UDC will work with developers to explore design approaches that maximize conformance with guidelines. The UDC shall not deny a Certificate of Appropriateness solely because a project fails to comply with a guideline.

3.D. Development Review Process

1. **PROJECTS SUBJECT TO REVIEW** – All private construction projects, with the exception of interior construction or exterior in-kind replacement work, are subject to review by the Planning and Development Department and/or the Urban Design Commission (UDC) for compliance with the NS Standards and Guidelines. All buildings, streets, and public spaces by public entities are also subject to review.
2. **ADMINISTRATIVE REVIEW VS. UDC REVIEW** – Projects that are clearly consistent with all applicable development principles and that clearly conform to all standards may be approved administratively by the Planning and Development Director without UDC review. The UDC shall review projects that require interpretation or discretionary judgment with respect to the project's compliance with standards and guidelines. The UDC shall also review all proposed drive-through facilities (see Section 5.D.5.).
3. **URBAN DESIGN COMMISSION (UDC)** – The UDC is a nine-member body appointed by the City Council and charged with the administration of urban design districts. As stated above, the UDC shall review projects that require interpretation or discretionary judgment with respect to the project's compliance with standards and guidelines. The Zoning Ordinance specifies that UDC appointees should be knowledgeable about fundamental principles of urban design. Furthermore, at least six of the nine appointees shall be practicing professionals from the fields of architecture, landscape architecture, urban design or planning, real estate, or law. The Planning and Development Director serves as staff advisor to the UDC.

3.D. Development Review Process (cont.)

4. **CERTIFICATES OF APPROPRIATENESS** – The Planning and Development Director or UDC will issue a Certificate of Appropriateness (COA) for approved public and private projects. In exceptional cases the UDC may approve a COA if a project is clearly consistent with applicable development principles but does not conform to a certain development standard(s) or guideline(s). The COA for such projects must describe the projects' compliance with applicable principles.
5. **REQUIRED INFORMATION** – The full list of required materials is included in the Near Southside District application available from the City's Planning and Development Department, 817-392-8000, www.fortworthgov.org/planning. In general, the following information is required, as applicable:
 - a. Site Plan
 - b. Building Plans and Elevations
 - c. Landscape Plan
 - d. Material Specifications
 - e. Plans and Specifications for Proposed Signs
 - f. Description of Proposed Scope of Work
 - g. Photographs of Site and Existing Conditions
6. **REVIEW OF PHASED PROJECTS** – To minimize review steps for phased development projects, the UDC may approve a COA for all phases provided that the applicant submits drawings that depict the initial phase as well as all future phases. The COA is valid for two years from the date of approval.
7. **CONCEPTUAL PHASE DISCUSSIONS** – One of the UDC's most important roles is to provide informal guidance to developers of significant projects during the conceptual design phase. These discussions take place during monthly UDC work sessions, and provide an opportunity for early UDC and staff input that should facilitate timely design review and approval of the final design phase. City staff encourages all public and private developers of significant projects to present their conceptual plans during a UDC work session.
8. **VARIANCES TO ZONING AND SUBDIVISION ORDINANCES' PROPERTY DEVELOPMENT STANDARDS**
 The UDC is authorized to approve appropriate exceptions to the vast majority of the NS Standards and Guidelines, provided a project complies with applicable development principles. The UDC may allow additional flexibility for projects of exceptional civic or environmental design. Any waiver of basic property development standards related to building heights or setbacks, however, requires a variance approved by the Board of Adjustment. Similarly, vacations of public rights-of-way and any waivers of subdivision requirements require approval by the City Plan Commission. Variances, vacations, and subdivision waivers should be recommended for approval by the UDC.

REVIEW OF PUBLIC PROJECTS



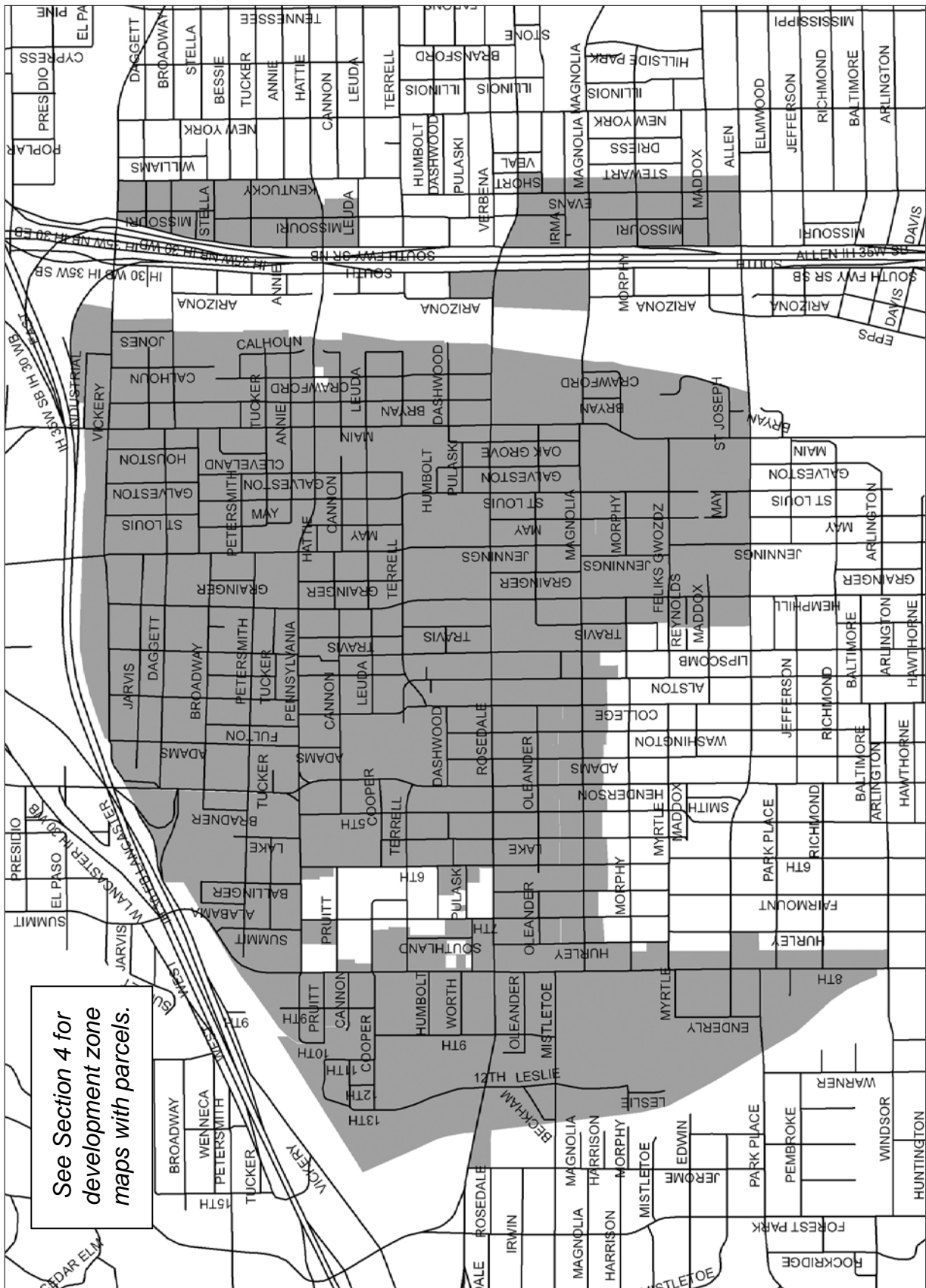
In addition to private construction projects, public projects such as street and streetscape improvements are subject to review for consistency with the standards and guidelines.

CONCEPTUAL DESIGN REVIEW



Developers are strongly encouraged to present conceptual plans for large and/or multi-phase projects at an early stage to facilitate timely review during the final design phase.

3.E. Near Southside District Boundary



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4.A. Intent and Principles

The Near Southside Regulating Plan functions similarly to both the Zoning Ordinance map and the Master Thoroughfare Plan map. The NS Regulating Plan includes maps that depict the boundaries of different types of development zones, as well as maps identifying the location and classification of thoroughfares. These maps serve as a guide for determining which development standards and guidelines apply to a development project. A unified section that classifies both development zones and streets is intended to promote coordination among land use, urban design, and transportation decisions, which is an essential strategy in creating a vibrant, livable, mixed-use district. The following principles guide the content of the Regulating Plan.

DEVELOPMENT ZONES

1. Draw development zone boundaries so as to enhance the character of existing neighborhoods and commercial districts while also promoting compatible higher-density, mixed-use redevelopment in appropriate locations.
2. Draw development zone boundaries to promote a large number of new residential units in order to create a “live, work, play” environment and to support neighborhood retail and other commercial uses.
3. Draw development zone boundaries to support large institutions and businesses that warrant tailored development standards based on their unique operational requirements.

HISTORIC PROPERTIES

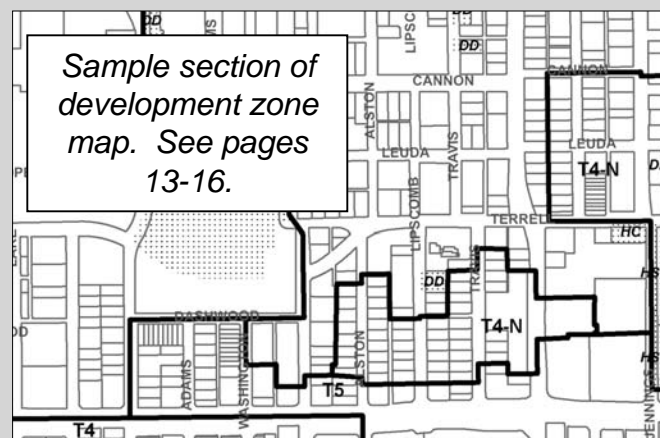
4. Facilitate the incorporation of historically significant buildings into redevelopment projects by identifying those properties on the Regulating Plan and by referencing the Zoning Ordinance’s Historic Preservation Ordinance and applicable incentives.
5. Facilitate the designation of currently unprotected historic properties and their adaptive reuse by identifying the most eligible properties on the Regulating Plan as information on those properties becomes available.

CIRCULATION NETWORK AND STREET CLASSIFICATION

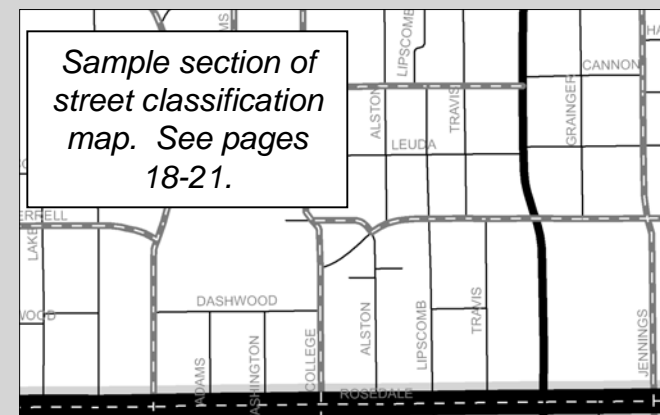
6. Preserve the existing urban street grid to maximize street connectivity for vehicles, pedestrians, public transportation, and bicycles. The grid promotes efficient circulation and provides a wide range of mobility options.
7. Limit vacations of existing public rights-of-way, including streets and alleys, only to situations where there is no adverse impact to future circulation and desirable redevelopment, or in cases where such vacations are absolutely necessary for an exceptional redevelopment project that is clearly consistent with the general development principles.
8. Utilize a context-sensitive street classification system that gives equal consideration to redevelopment and mobility goals.

HOW TO USE THE REGULATING PLAN

1. Locate the subject property on the applicable development zone and historic properties map in Section 4.B.



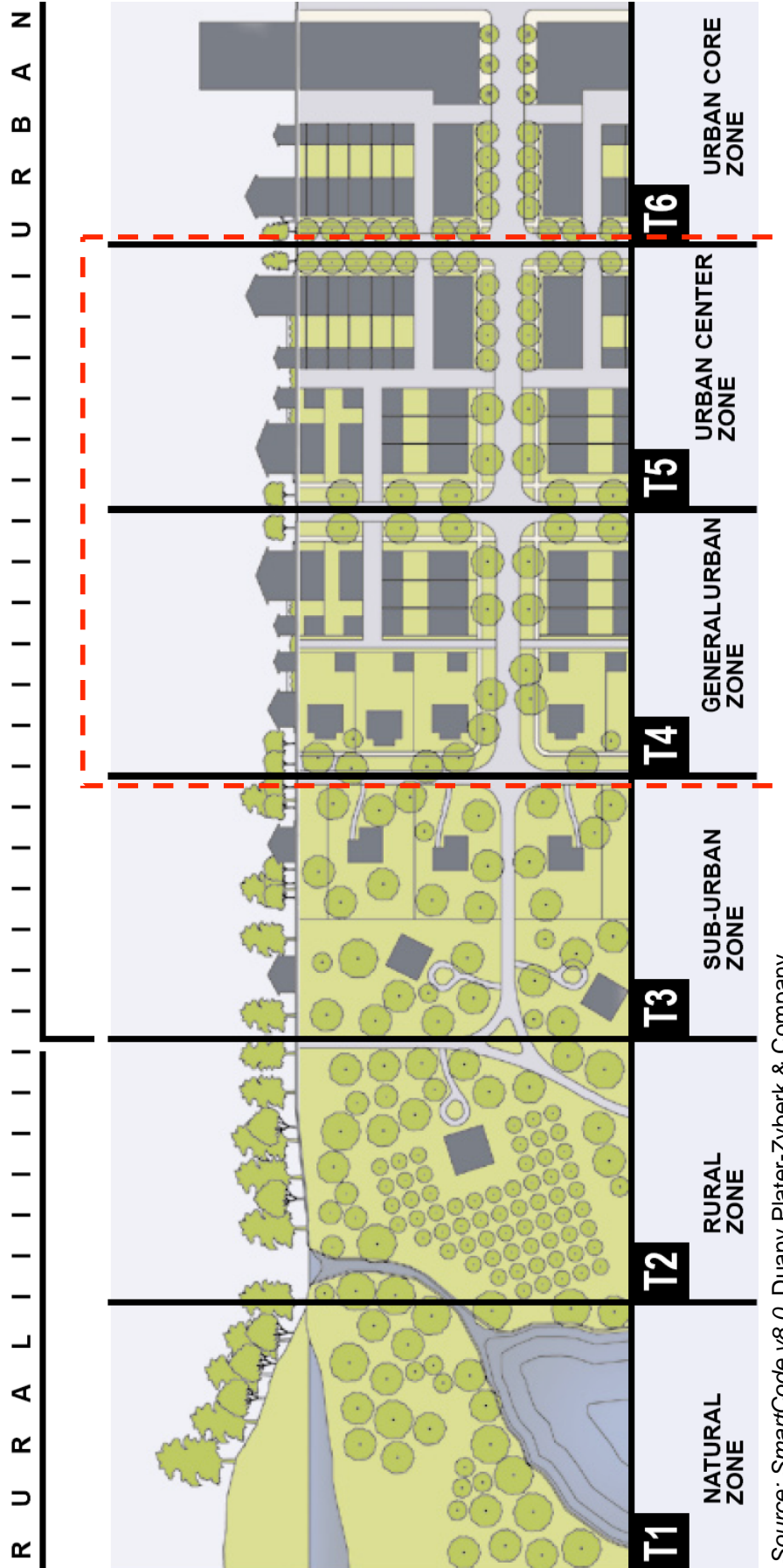
2. Identify the property’s development zone and any applicable historic designations.
3. Locate the subject property on the applicable circulation network and street classification map in Section 4.C.



4. Identify the classifications of all adjacent streets.
5. Refer to Sections 5-7 for applicable standards and guidelines related to streets, streetscapes, public spaces, sites, and buildings.
6. Refer to Chapter 4, Article 5 of the Zoning Ordinance for regulations pertaining to historic properties.
7. Refer to all other referenced sections of the Zoning Ordinance or other regulatory manuals.

The Transect Classification System

The diagram below illustrates the "Transect" that serves as the basis for many form-based development codes. The Transect diagram shows a continuous cross-section of human habitats, from rural to urban. Transect zones "T4: General Urban" and "T5: Urban Center" are most consistent with the redevelopment vision for the Near Southside. (The development standards for T4 and T5 are very similar to those for the MU-1 and MU-2 zoning districts, respectively.) T4 and T5 zones, as well as special neighborhood and institutional/industrial zones are shown on the development zone maps in the following section.



Source: SmartCode v8.0, Duany Plater-Zyberk & Company

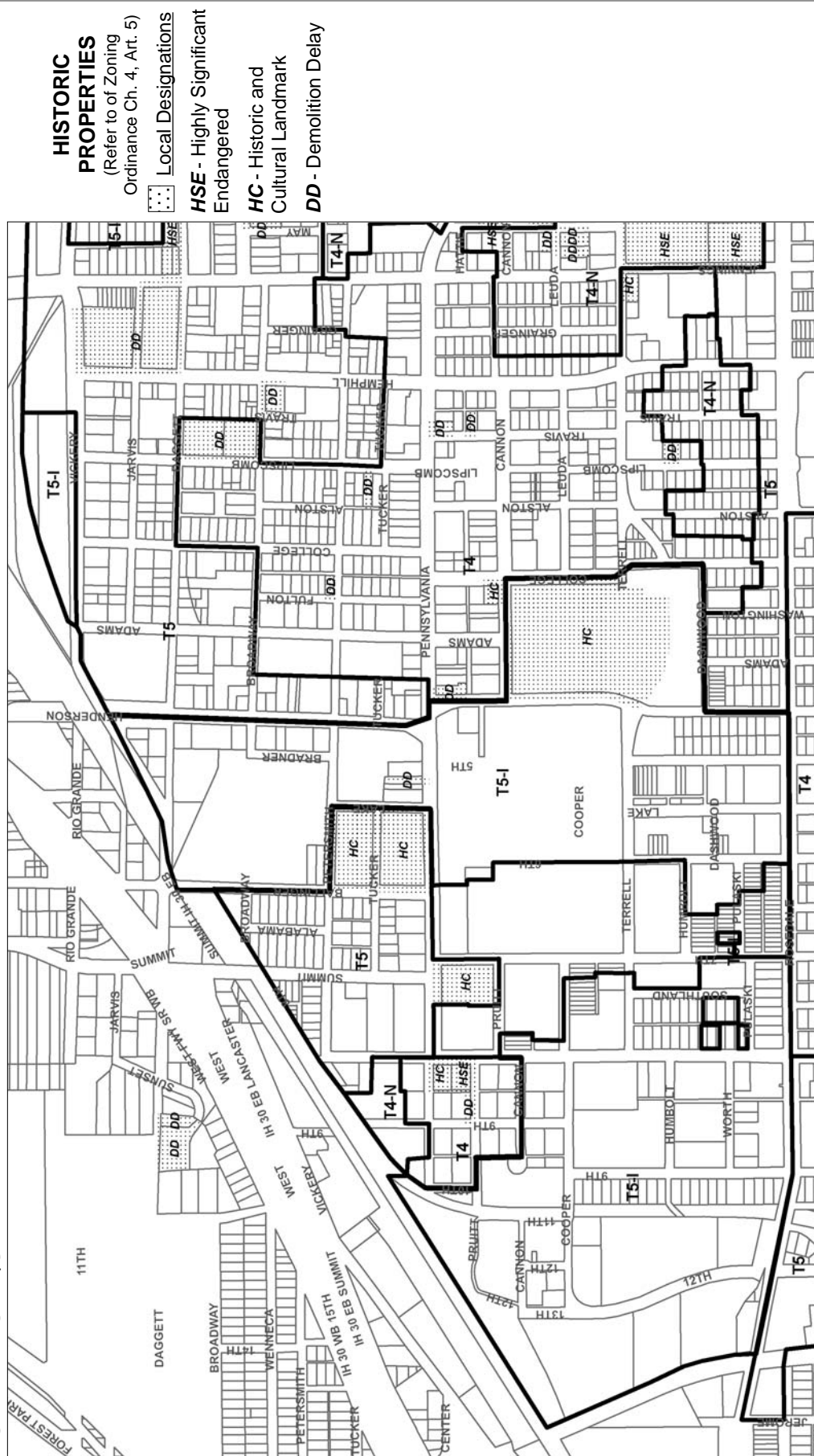
4.B. Development Zones and Historic Properties

1. NORTHWEST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4	20-ft. maximum front setback 18-foot minimum façade height 3 stories max. if single use 5 stories max. w/ public space <u>or</u> mix of uses 6 stories max. w/ public space <u>and</u> mix of uses Parking behind or beside buildings	-N
T5	20-foot maximum front setback 18-foot minimum façade height 5 stories max. if single use 8 stories w/ public space <u>or</u> mix of uses 10 stories w/ public space <u>and</u> mix of uses Parking behind or beside buildings	-I
		<u>Neighborhood zone</u> No single-use non-residential 3 stories max., 4 stories w/ public space or mix of uses Institutional/Industrial zone Flexible driveway and entrance standards

NORTHWEST QUADRANT



4.B. Development Zones and Historic Properties

2. SOUTHWEST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4

20-ft. maximum front setback
18-foot minimum façade height
3 stories max. if single use
5 stories max. w/ public space or mix of uses
6 stories max. w/ public space and mix of uses
Parking behind or beside buildings

T5

20-foot maximum front setback
18-foot minimum façade height
5 stories max. if single use
8 stories w/ public space or mix of uses
10 stories w/ public space and mix of uses
Parking behind or beside buildings

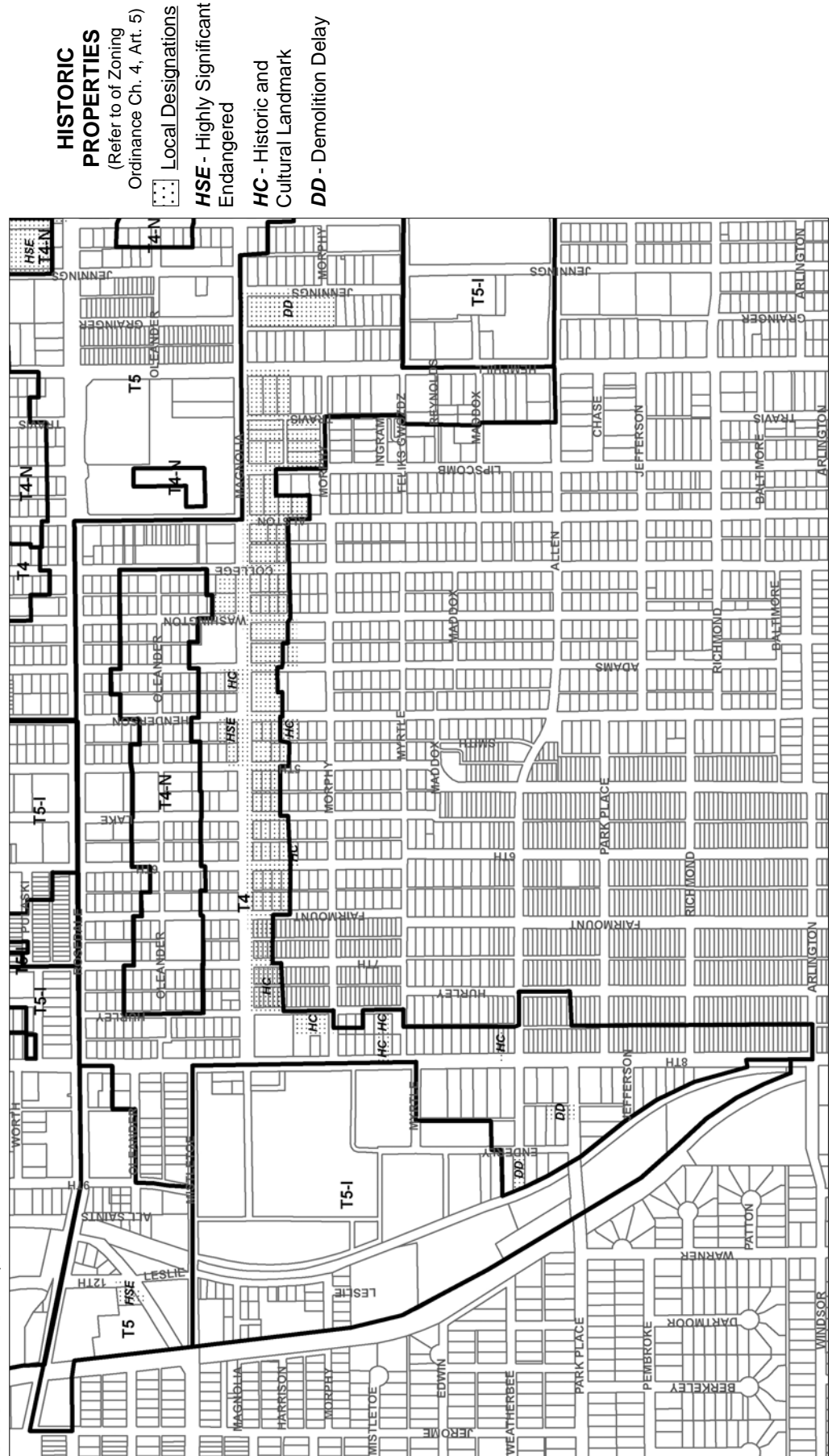
-N

Neighborhood zone
No single-use non-residential
3 stories max., 4 stories w/ public space or mix of uses

-I

Institutional/Industrial zone
Flexible driveway and entrance standards

SOUTHWEST QUADRANT



4.B. Development Zones and Historic Properties

3. NORTHEAST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4

- 20-ft. maximum front setback
- 18-foot minimum façade height
- 3 stories max. if single use
- 5 stories max. w/ public space or mix of uses
- 6 stories max. w/ public space and mix of uses
- Parking behind or beside buildings

T5

- 20-foot maximum front setback
- 18-foot minimum façade height
- 5 stories max. if single use
- 8 stories w/ public space or mix of uses
- 10 stories w/ public space and mix of uses
- Parking behind or beside buildings

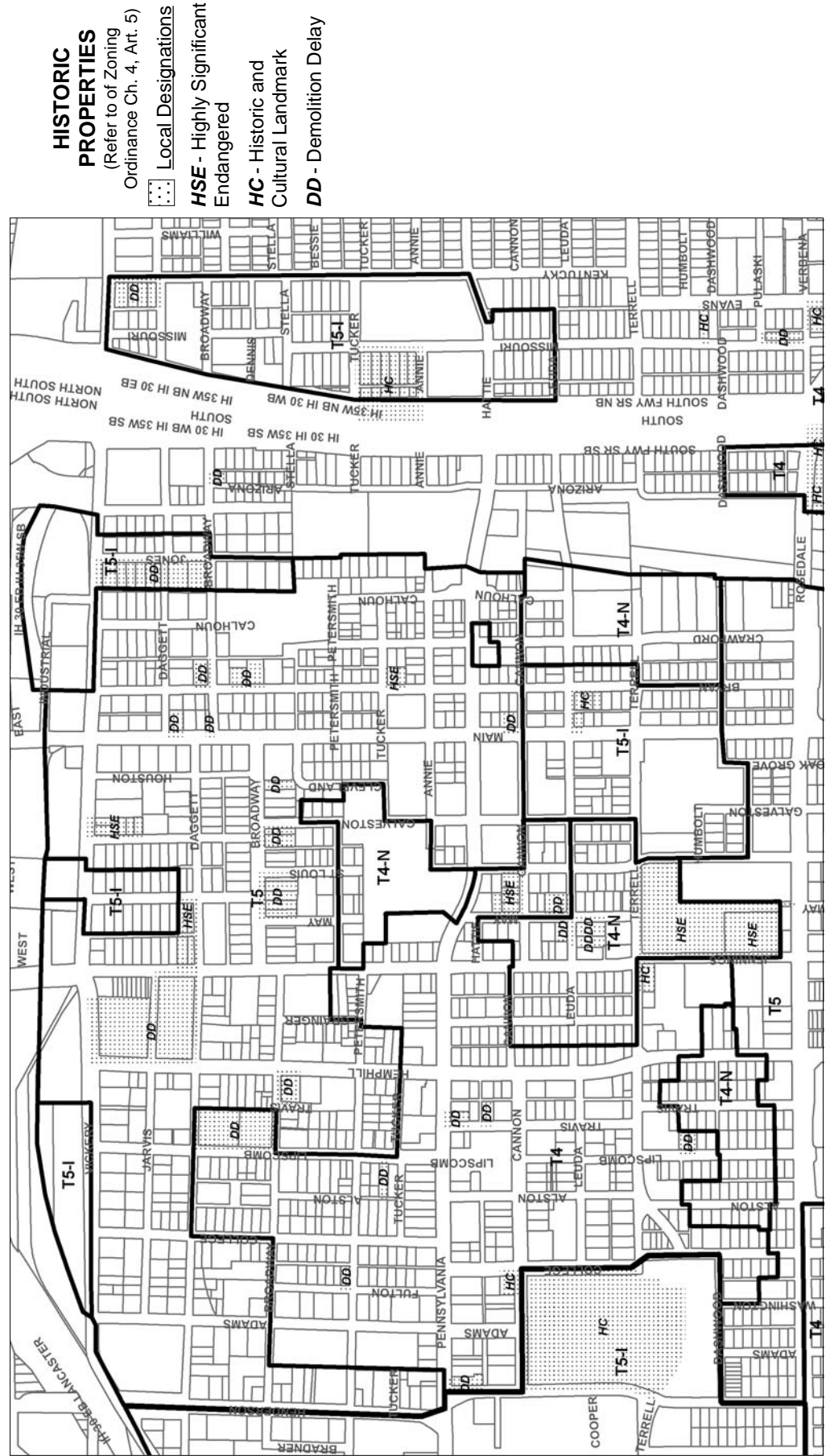
-N

- Neighborhood zone
- No single-use non-residential
- 3 stories max., 4 stories w/ public space or mix of uses

-I

- Institutional/Industrial zone
- Flexible driveway and entrance standards

NORTHEAST QUADRANT



4.B. Development Zones and Historic Properties

4. SOUTHEAST

Basic Development Standards (Standards and guidelines are listed in Sections 5-7.)

T4

20-ft. maximum front setback
18-foot minimum façade height
3 stories max. if single use
5 stories max. w/ public space or mix of uses
6 stories max. w/ public space and mix of uses
Parking behind or beside buildings

T5

20-foot maximum front setback
18-foot minimum façade height
5 stories max. if single use
8 stories w/ public space or mix of uses
10 stories w/ public space and mix of uses
Parking behind or beside buildings

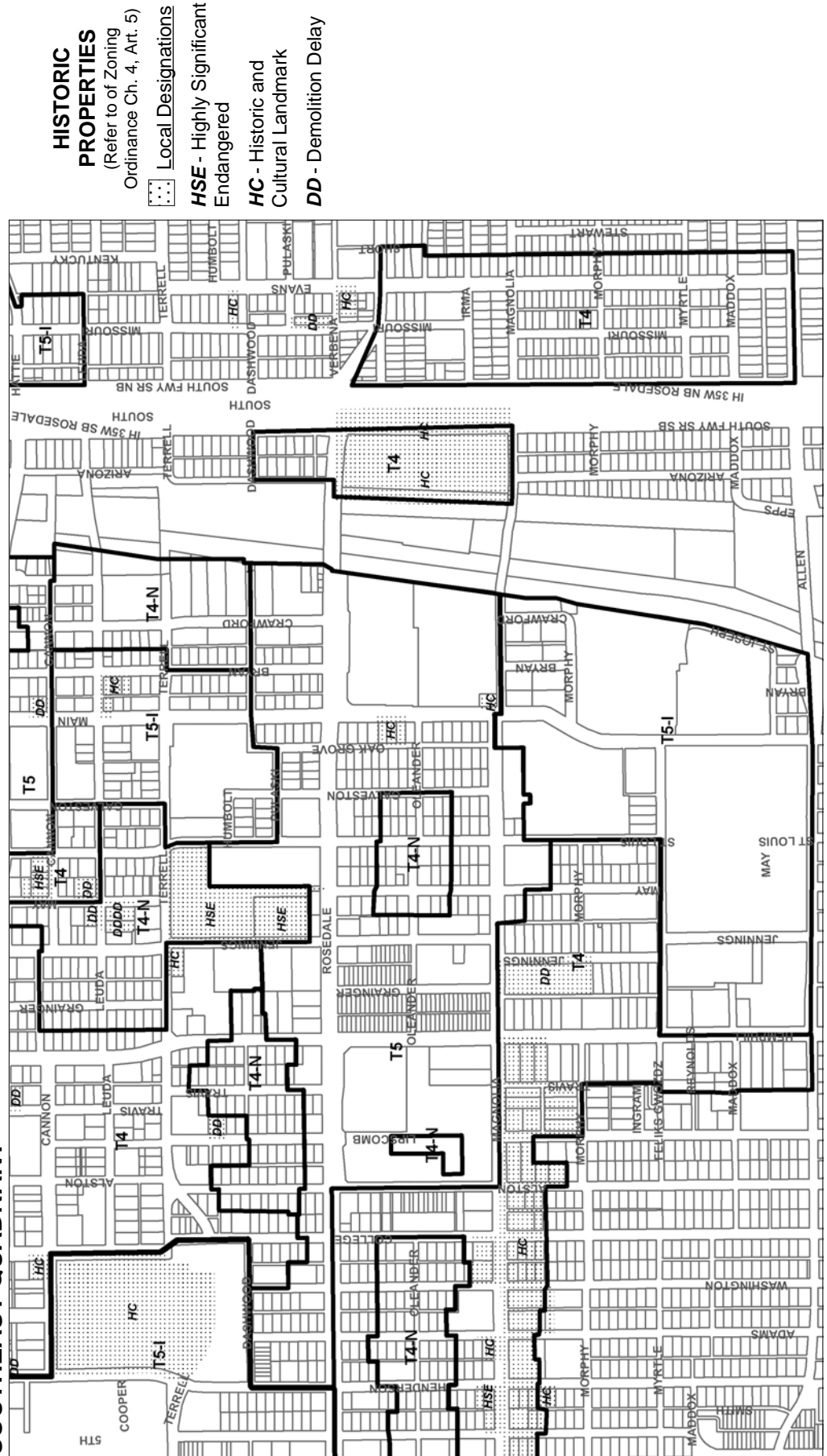
-N

Neighborhood zone
No single-use non-residential
3 stories max., 4 stories w/ public space or mix of uses

-I

Institutional/Industrial zone
Flexible driveway and entrance standards

SOUTHEAST QUADRANT



4.C. Circulation Network and Thoroughfare Classification

1. CLASSIFICATIONS BY CONTEXT: DEFINITIONS

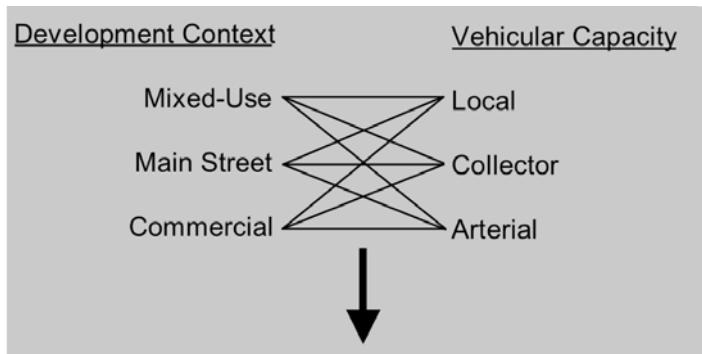
- Mixed-Use Streets** – Flexible and walkable urban streets serving a variety of land uses and densities. These include on-street parking and comfortable sidewalks, and are comfortable and safe for bicyclists.
- Main Streets** – Walkable, mixed-use streets of moderate to high density. These are primary destination streets with active ground floor uses and on-street parking, and are comfortable and safe for bicyclists.
- Commercial Streets** – Automobile-oriented streets with high levels of automobile and truck movement, no on-street parking, and low to moderate levels of pedestrian and bicycle movement.

2. CLASSIFICATIONS BY VEHICULAR CAPACITY: DEFINITIONS

- Local Streets** – less than 2,500 vehicles per day.
- Collector Streets*** – less than 8,000 vehicles per day.
- Arterial Streets** – more than 8,000 vehicles per day.
 - Minor* – Usually undivided with 3-4 travel lanes
 - Major* – Usually divided with 4 travel lanes
 - Principal – Usually divided with 6 travel lanes

*If feasible, striped bike lanes are usually necessary on these streets to create a safe and comfortable cycling environment.

3. CONTEXT-SENSITIVE STREET CLASSIFICATIONS



Context-Sensitive Street Classifications (Examples)

Mixed-Use Local (North-south streets at Oleander Walk)
 Mixed-Use Collector (Jennings north of Rosedale)
 Mixed-Use Arterial (8th Avenue)

Main Street Local (Park Place)
 Main Street Collector (Magnolia)
 Main Street Arterial (West Rosedale between 8th Ave. and S. Main)

Commercial Local (No Near Southside examples)
 Commercial Collector (Vickery across I-35W)
 Commercial Arterial (West Rosedale, west of 8th Ave. and east of S. Main)

CONTEXT-SENSITIVE CLASSIFICATIONS



Mixed-Use Streets are walkable and serve a variety of residential and non-residential land uses.

















Main Streets are the primary retail and destination streets.



Commercial Streets are oriented to vehicles but still provide comfortable pedestrian environments. (West Rosedale rendering by Carter & Burgess)

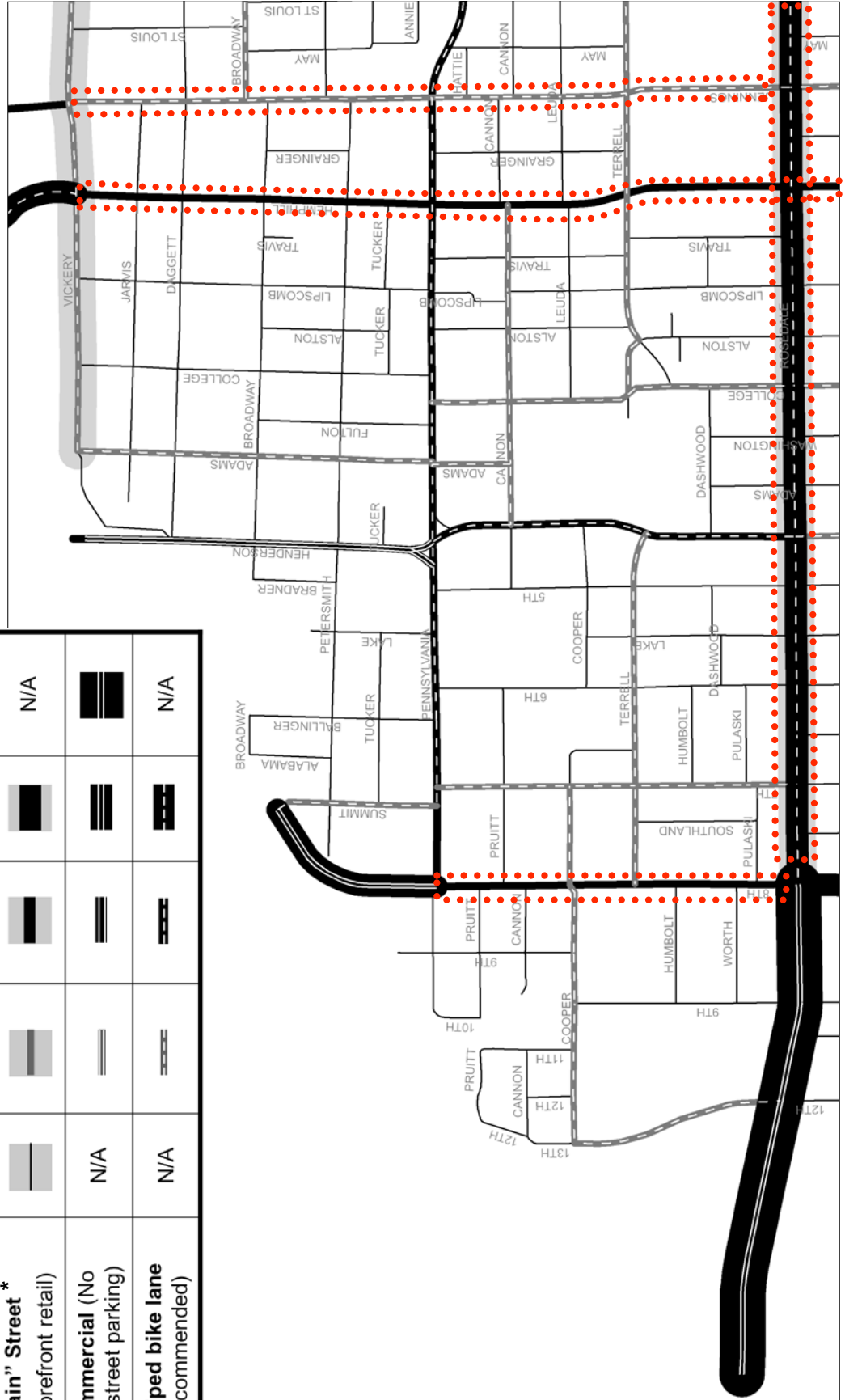
4.C. Circulation Network and Thoroughfare Classification

4. NORTHWEST QUADRANT

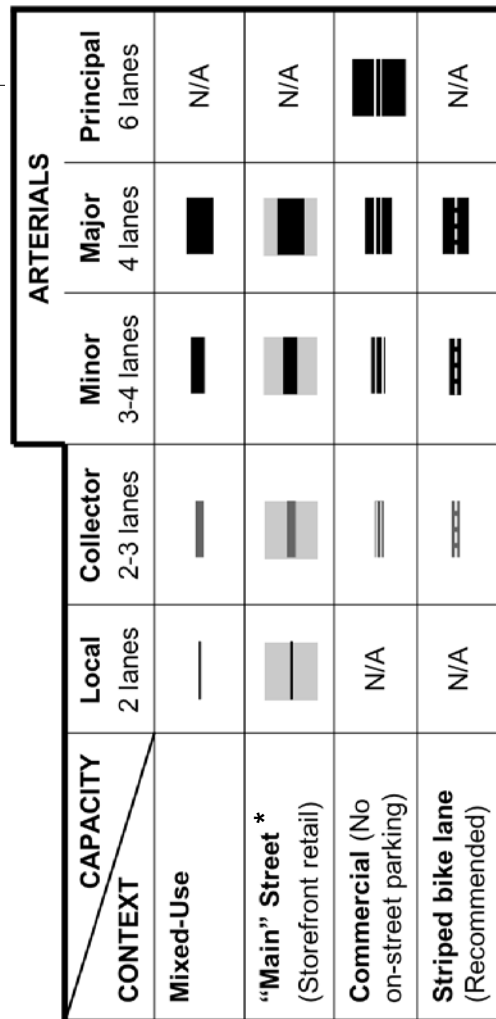
CAPACITY CONTEXT		ARTERIALS				
		Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes	Principal 6 lanes
Mixed-Use		—				N/A
“Main” Street * (Storefront retail)						N/A
Commercial (No on-street parking)		N/A				
Striped bike lane (Recommended)		N/A				N/A


* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
5.C.5.c. 5.E.8. 5.F.5.d.
5.F.5.f.

— Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.



5. SOUTHWEST QUADRANT



 Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.

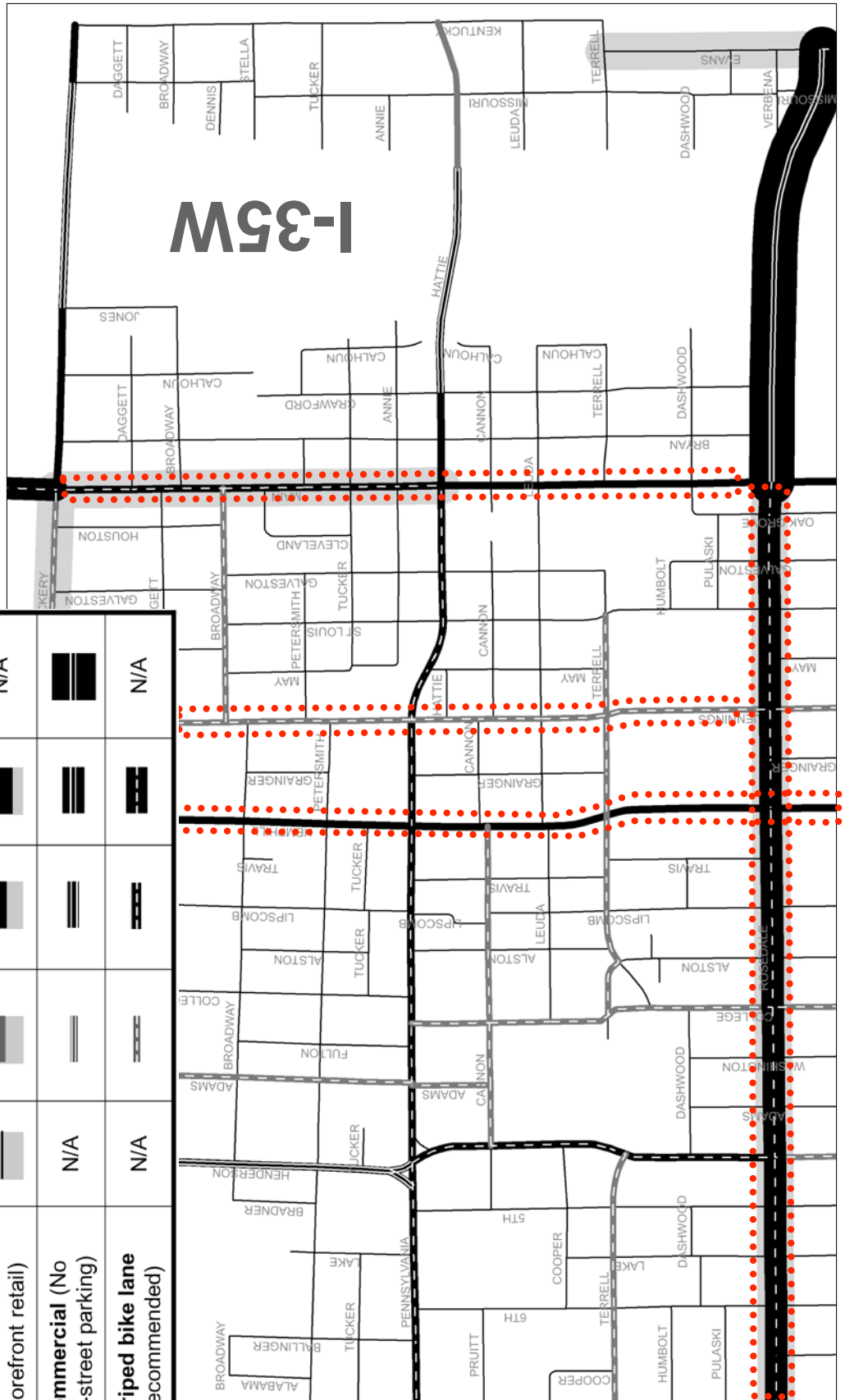
4.C. Circulation Network and Thoroughfare Classification

6. NORTHEAST QUADRANT

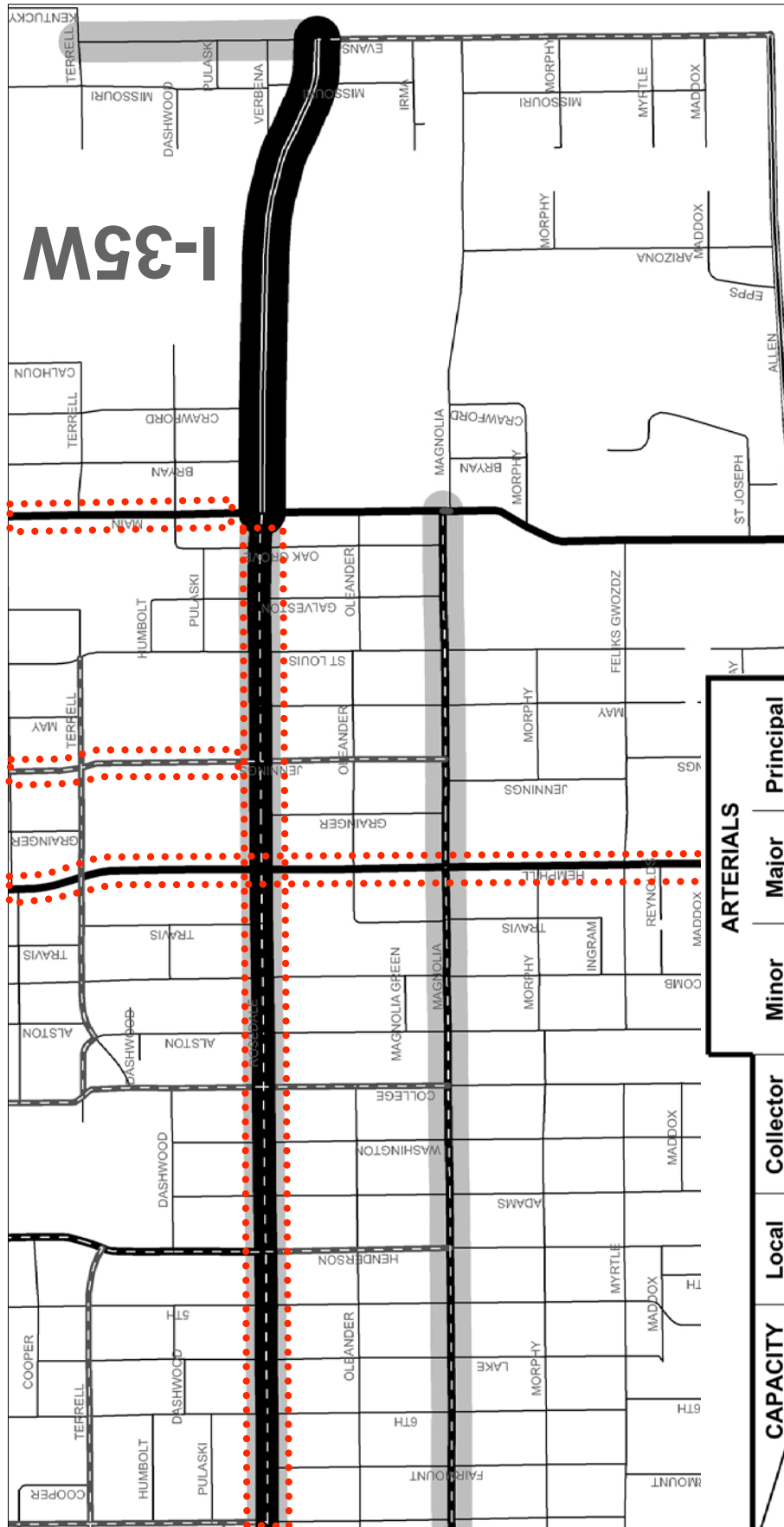
CAPACITY CONTEXT	ARTERIALS			
	Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes
Mixed-Use	—	—	—	—
"Main" Street * (Storefront retail)	—	—	—	—
Commercial (No on-street parking)	N/A	—	—	—
Striped bike lane (Recommended)	N/A	—	—	—

* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
5.C.5.c. 5.E.8. 5.F.5.d.
5.F.5.f.

Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.


















7. SOUTHEAST QUADRANT



* See: 5.B.2.b. 5.B.3.d. 5.C.2.a.
5.C.5.c. 5.E.8. 5.F.5.d.
5.F.5.f.

Dashed line indicates that the proposed capacity classification is currently inconsistent with the Master Thoroughfare Plan. Amendment under discussion.

CAPACITY CONTEXT		ARTERIALS				
		Local 2 lanes	Collector 2-3 lanes	Minor 3-4 lanes	Major 4 lanes	Principal 6 lanes
Mixed-Use						N/A
“Main” Street (Storefront retail)						N/A
Commercial (No on-street parking)		N/A				
Striped bike lane (Recommended)		N/A				N/A

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5.A. Intent and Principles

This section includes standards and guidelines related to the orientation and configuration of streets, public spaces, buildings, sites, and parking facilities. The purpose of these standards and guidelines is to promote high quality, pedestrian-oriented, sustainable development that adapts to changing conditions over time, without dictating architectural style. The following principles provide the basis for the development standards and guidelines.

STREETS AND PUBLIC SPACES

1. Design all streets and sidewalks to promote pedestrian activity and comfort. Sidewalks serve as the Near Southside's most heavily used public spaces.
2. Balance the circulation requirements of automobiles, mass transit vehicles, bicycles, and pedestrians in the design of street cross-sections. Utilize context-sensitive design strategies to achieve this balance.
3. Provide designated road space for bicycles, such as striped bike lanes, on roads that would otherwise be uncomfortable or unsafe for less experienced or slow riders.
4. Emphasize attention to detail and quality construction in the design and construction of streets.
5. Use simple but effective roadside designs to allow easy maintenance.
6. Provide on-street parking in as many areas as possible to support district businesses and to calm traffic speeds.
7. Adhere to time-tested roadside design strategies that create walkable streets, including shade trees and pedestrian lights located along the curb, between the roadway and the walkway.
8. Provide a wide range of public spaces (in addition to comfortable sidewalks), including neighborhood-oriented pocket parks, community gathering places, and recreational facilities.
9. Incorporate elements into public spaces that engage all age groups, including young children and the elderly.



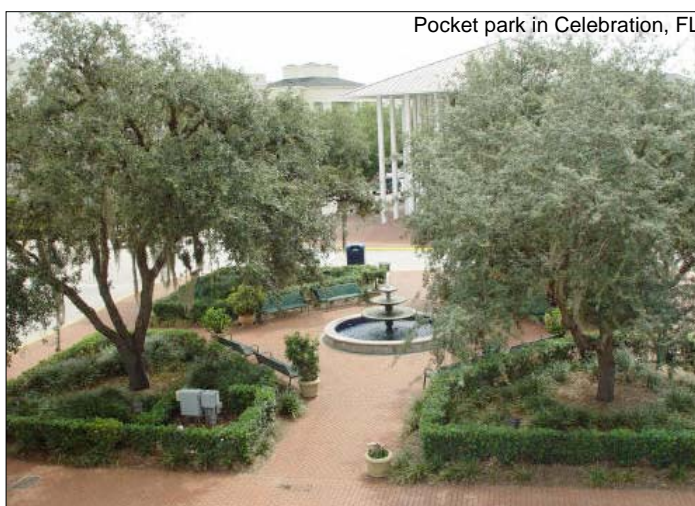
Magnolia Avenue

On-street parking, wider sidewalks, street trees, and outdoor dining areas will help create the district's most appealing and heavily used public spaces.



Downtown Plano

On-street parking supports retail and restaurants and also provides a traffic buffer for pedestrians.



Pocket park in Celebration, FL

A network of public spaces that includes both smaller plazas and larger parks is a key element in urban districts.

5.A. Intent and Principles (cont.)

SITES AND BUILDINGS

9. Design sites and buildings, and the lower floors of buildings in particular, so that they promote pedestrian activity and provide an active, human-scale, continuous, pedestrian-oriented street edge along public sidewalks.
10. Design buildings and sites that provide visual variety and enhance the Near Southside's overall sense of place.
11. Build distinctive neighborhoods that enhance local identity.
12. Promote building scale that enhances the public realm, and promote new buildings that complement the scale of neighboring structures, particularly adjacent historic buildings.
13. Locate and design surface parking lots, individual garages, and large parking garages so as to reduce their visual impact and/or to create public amenities (e.g., through aesthetic or environmental features).
14. Design and orient buildings on Magnolia, Park Place, West Rosedale, South Main, Pennsylvania, 8th Avenue, and Vickery so as to support the streets' roles as the Near Southside's primary pedestrian-oriented commercial districts.
15. Encourage the use of durable, attractive materials that promote sustainability and the reuse of buildings.
16. Design buildings that reflect the time in which the structures are built, as the district's most treasured historic buildings do.
17. Without dictating architectural style, encourage building facade designs that contribute to an attractive streetscape and skyline.
18. Encourage all civic and publicly sponsored buildings to be of exceptional design quality.
19. Design buildings and sites so as to minimize negative impacts on air and water quality and to promote a sustainable natural environment.



Storefronts along 8th Avenue

The principles support active streets with ground level uses that are oriented to public sidewalks.



Parking lot located to the side of a historic building on S. Main St.

Parking lots located behind or to the side of buildings are consistent with the district's urban character.



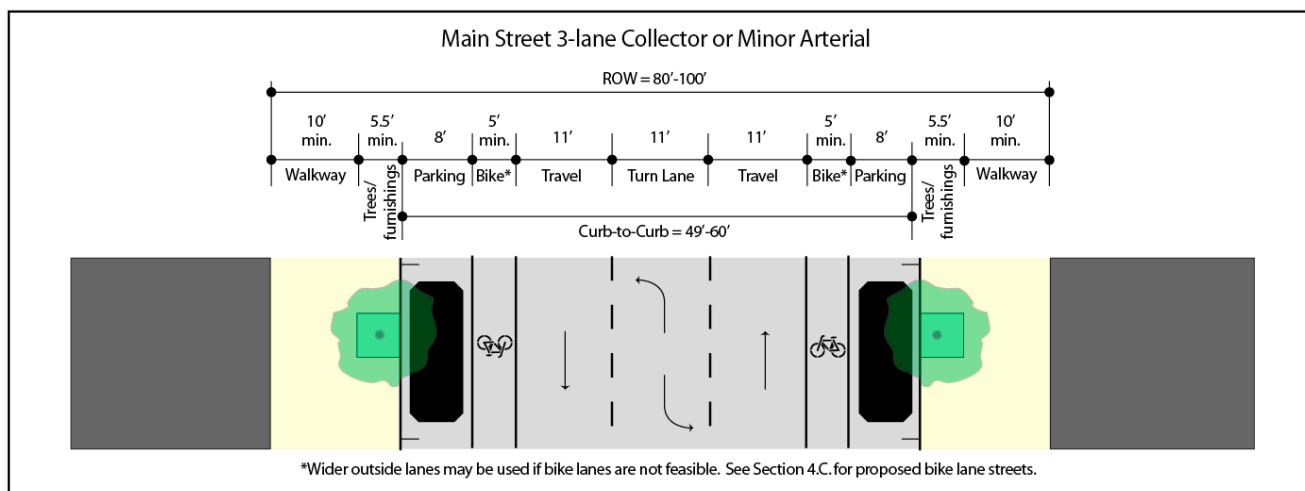
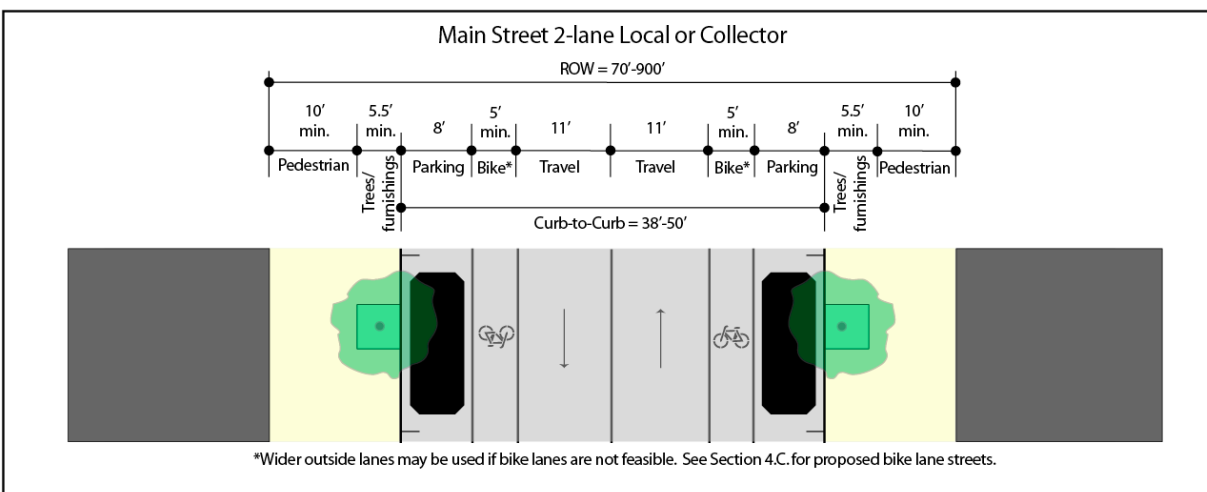
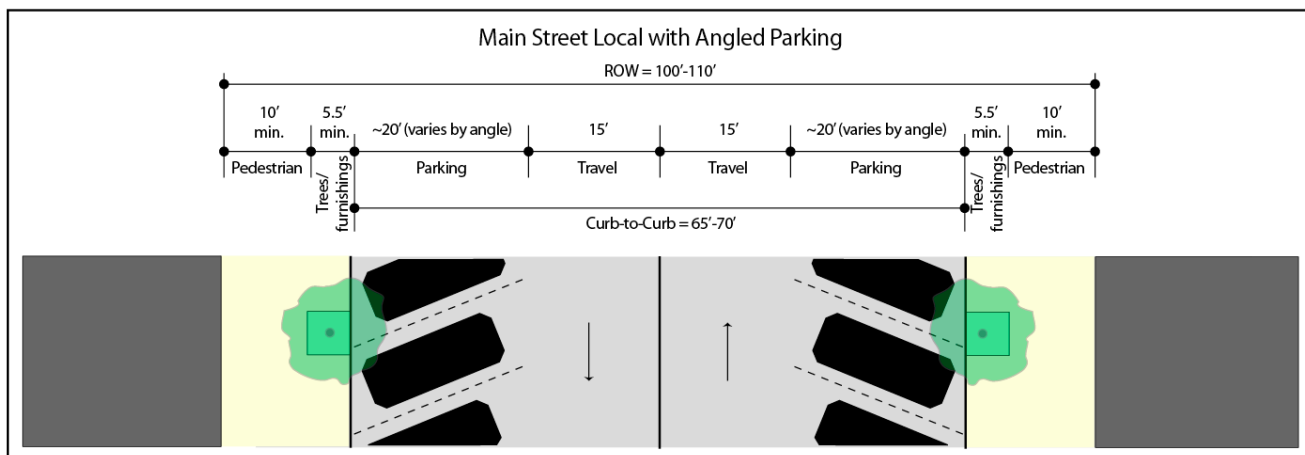
Historic Mehl Building on Magnolia Avenue

Historically significant buildings help define the district and should be incorporated into new projects.

5.B. Streets and Other Public Spaces

1. STREET STANDARDS

a. Main Streets



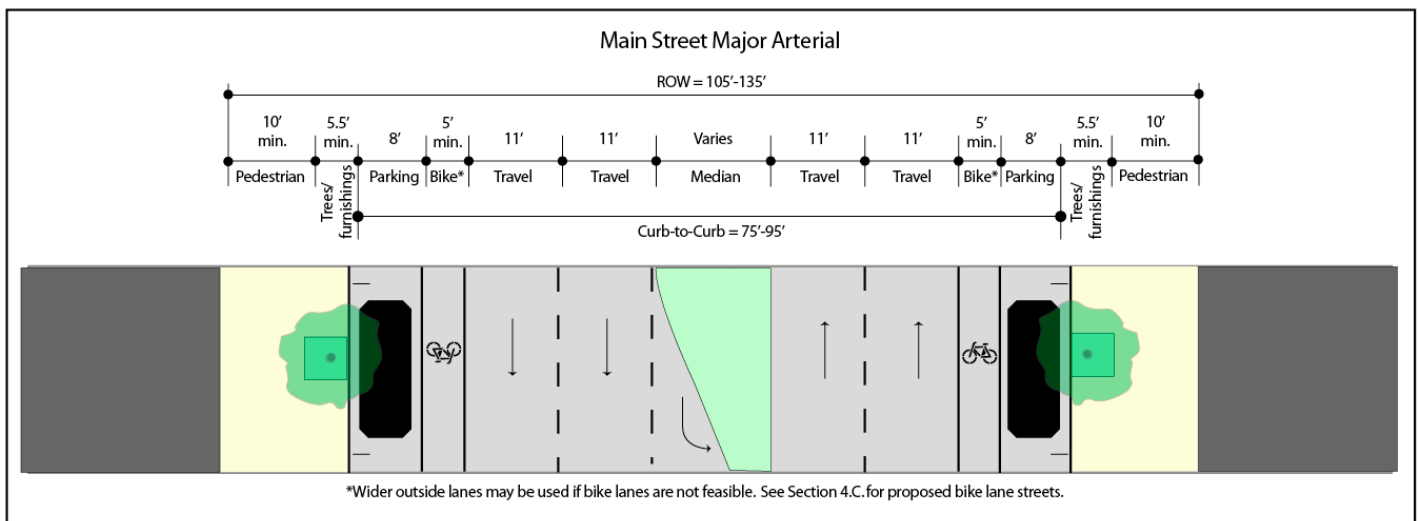
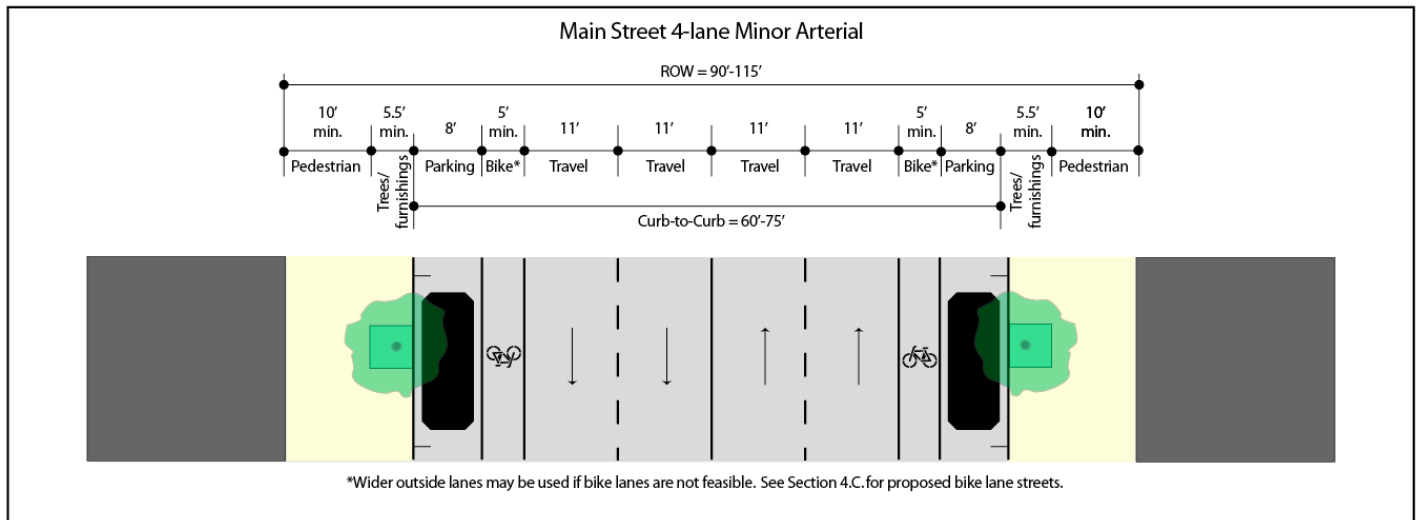
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

a. Main Streets (cont.)



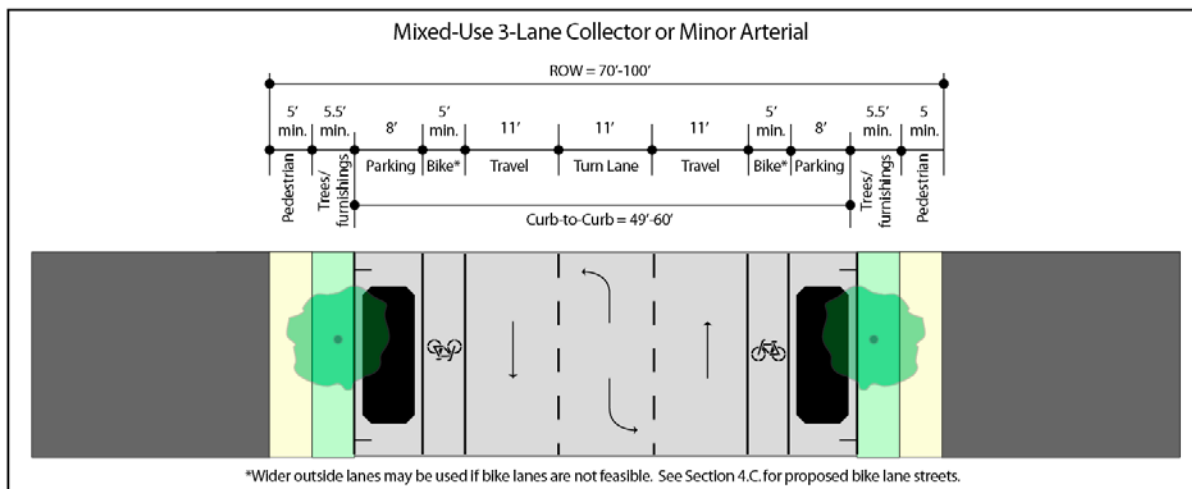
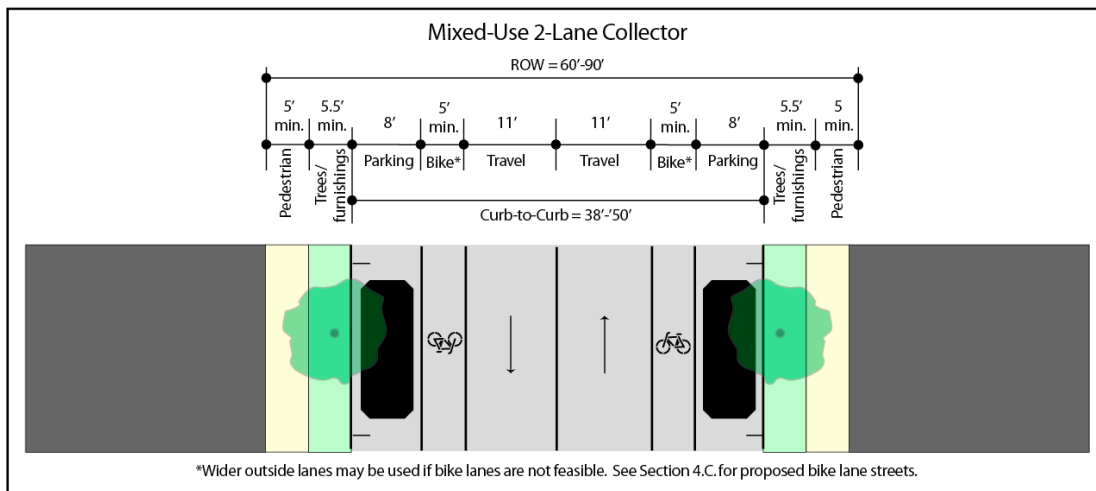
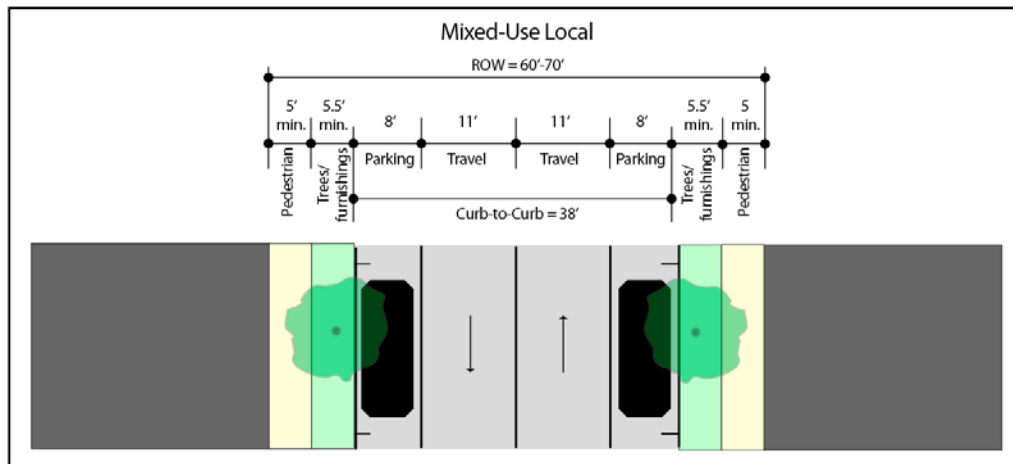
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5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

b. Mixed-Use Streets



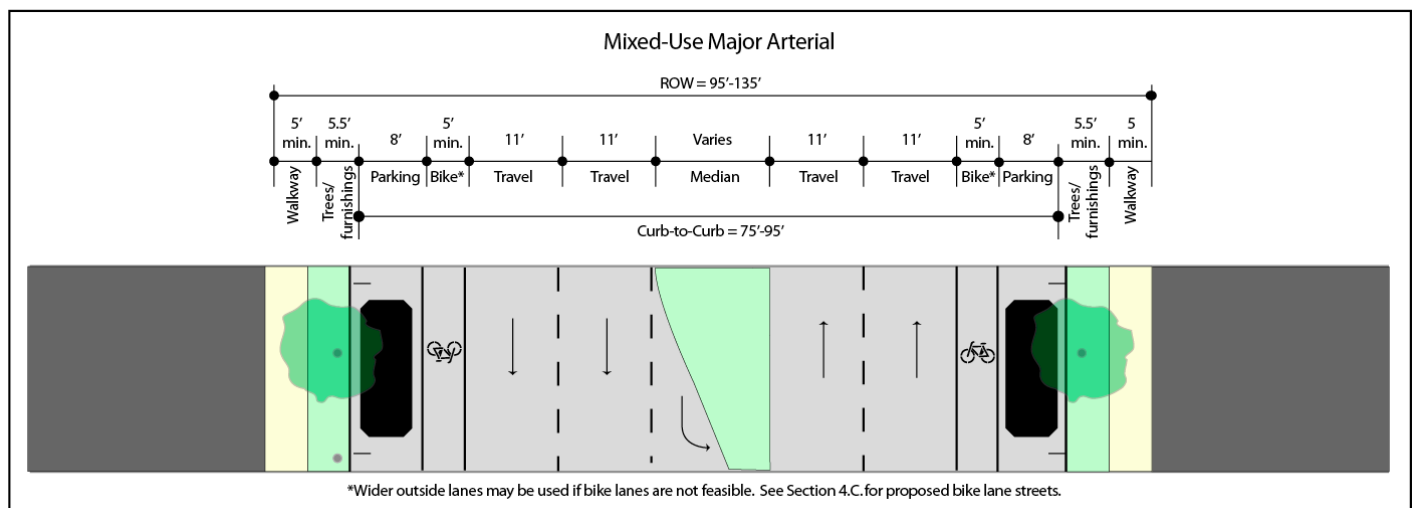
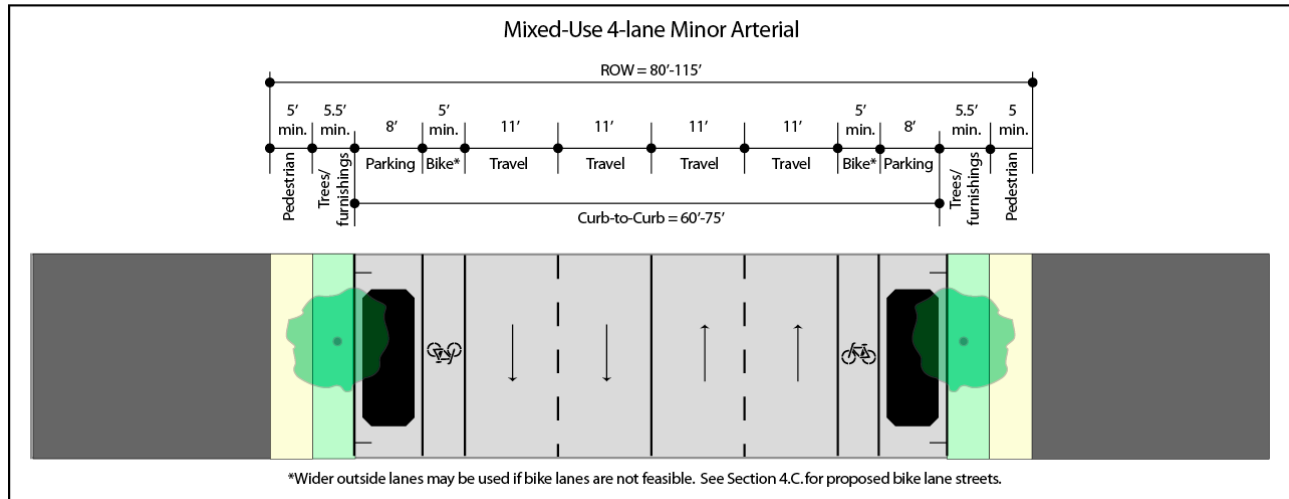
NOTES:

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- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

b. Mixed-Use Streets (cont.)



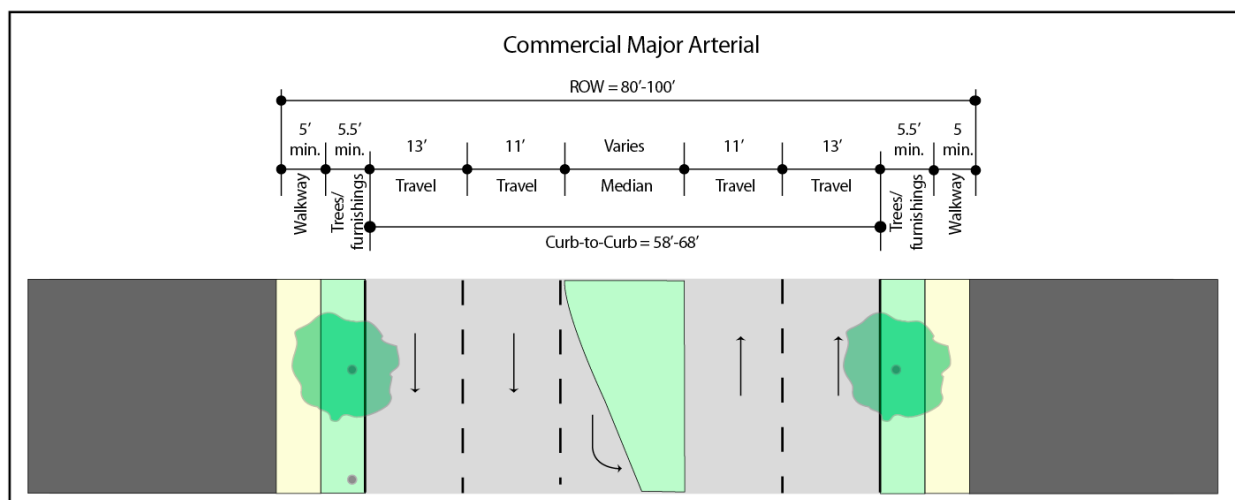
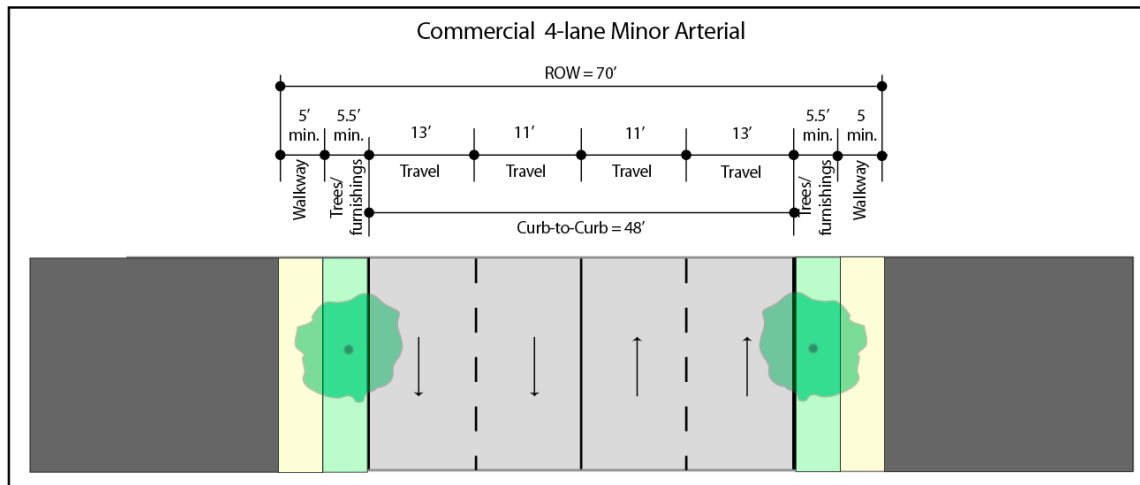
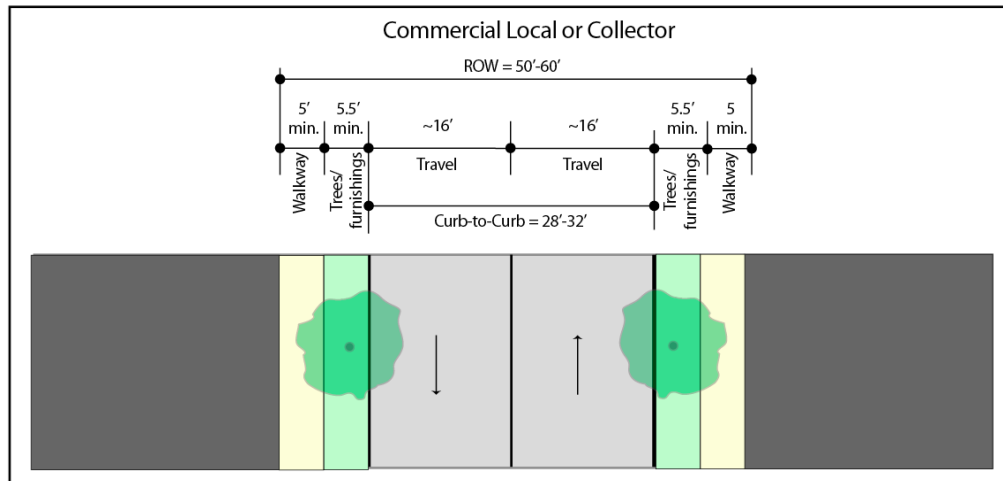
NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

c. Commercial Streets



NOTES:

- 1) The Near Southside street standards are based on the City's draft Context-Sensitive Solutions (CSS) Policy. In the case of any conflict between the NS and CSS standards (upon their adoption), the CSS standards shall apply.
- 2) The City's Traffic Engineer is authorized to approve modifications to cross-section dimensions in response to site-specific constraints.

5.B. Streets and Other Public Spaces (cont.)

1. STREET STANDARDS

d. Alleys

1) Alleys shall conform to all standards contained in the Subdivision Ordinance. Chapter 6, Article VI, Section G.2. of the Subdivision Ordinance includes the following dimensional requirements:

- Alleys serving single-family (including townhouses) or two-family residential:

Right-of-way width: Minimum 16 feet

Pavement width: Minimum 12 feet

- Alleys serving other uses:

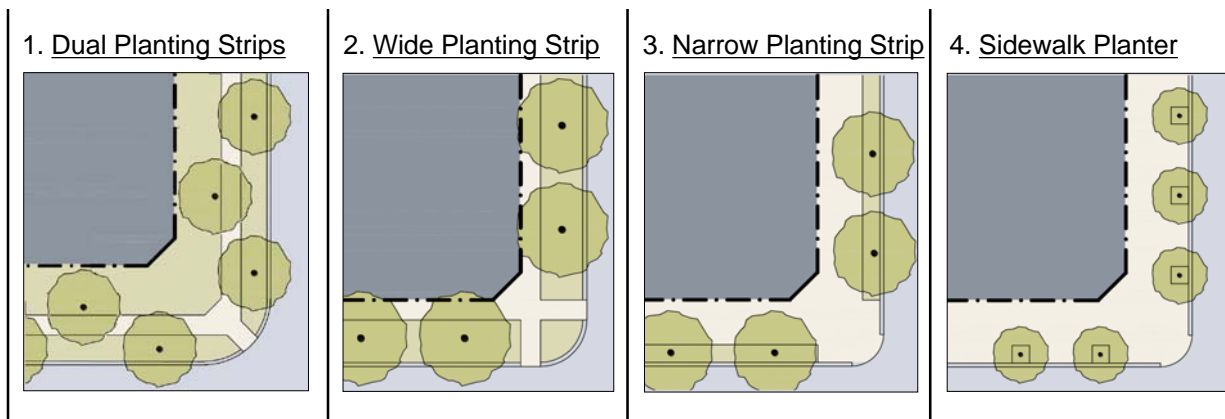
Right-of-way width: Minimum 20 feet

Pavement width: Minimum 20 feet

5.B. Streets and Other Public Spaces (cont.)

2. PUBLIC ROADSIDES

a. Roadside Design Options



Suitable Streets – Listed in order of applicability; see Thoroughfare Classification Maps in section 4.C.

1. Commercial Streets 2. Mixed-Use Streets	1. Mixed-Use Streets 2. Commercial Streets	1. Mixed-Use Streets 2. Main Streets	1. Main Streets 2. Mixed-Use Streets
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Roadside Width – Total width of pedestrian way and street tree/furniture zone (see 5.B.2.b. for minimums).

~20+ feet	~13-15 feet	~10-15 feet	~10-20 feet
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Pedestrian Way

Paved walkway	5 ft. minimum	5 ft. minimum	5-10 ft.	10-15 ft.
Walkway location	Between planting strips	Along property line	<ul style="list-style-type: none"> Along property line, or Along planting strip if sidewalk seating or display is located along property line 	<ul style="list-style-type: none"> Along property line, or Along planting strip if sidewalk seating or display is located along property line

Street Tree/Furniture Zone – Located along the curb; includes street trees, pedestrian lights, and other elements.

Planter	Min. 5 ft. wide (each)	Min. 8 ft. wide	Min. 5 ft. wide	Min. 5 ft. x 5 ft. tree well
Tree spacing (Sec. 3.a)	25-35 feet (Must be ≥10' from bldg.)	15-35 feet	15-35 feet	15-35 feet
Pedestrian light spacing (Sec. 3.c)	60 feet maximum	60 feet maximum	60 feet maximum	60 feet maximum
Other elements (Guidelines in Sec. 3.d.)	<ul style="list-style-type: none"> Irrigated groundcover Fire hydrants 	<ul style="list-style-type: none"> Irrigated groundcover Fire hydrants 	<ul style="list-style-type: none"> Irrigated groundcover Bike racks Trash bins Benches Wayfinding signs Sidewalk tables and displays 	<ul style="list-style-type: none"> Bike racks Trash bins Benches Wayfinding signs Sidewalk tables and displays

5.B. Streets and Other Public Spaces (cont.)

2. PUBLIC ROADSIDES (cont.)

b. Minimum roadside widths for new construction projects

To accommodate the required minimum widths for street tree/furniture zones and pedestrian ways, the following minimum roadside width standards apply. These standards apply regardless of the available public right-of-way. Where insufficient right-of-way exists, the pedestrian way shall extend onto private property, with a public sidewalk easement provided. The roadside shall be measured from the back of the curb to the outside edge of the pedestrian way (closest to the building façade).

1. "Commercial" and "Mixed-Use" streets – 10.5 ft. minimum
2. "Main" streets – 15 ft. minimum

3. ROADSIDE ELEMENTS

a. Street trees

1. Developments must conform to the tree preservation standards set forth in Section 5.G.1. The provisions below also apply.
2. Public and private development shall provide shade trees within the street tree/furniture zone.
3. Street trees shall be spaced 15-35 feet apart on center, depending on the trees' size at maturity. (See the list at right for spacing standards.) Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.
4. To maximize survival rates, trees shall be planted to ensure proper drainage and shall be irrigated.
5. Low maintenance tree planter systems, such as pervious pavers or planting strips, shall be used. Tree grates collect trash and damage trees and are not a low maintenance system.
6. Street trees shall be pruned to allow 8-foot clearance for pedestrians.
7. Trees shall be planted at the time of development, although the planting schedule may be extended to allow planting during the fall/winter season.

b. Pedestrian Way

1. The pedestrian way shall have walkways a minimum width of 5 feet and shall comply with ADA and Texas Accessibility Standards. Exemption: Minor renovation projects on properties with existing 4 ft. sidewalks.
2. Paving materials shall be attractive and easy to maintain. Appropriate materials:
 - Concrete
 - Pervious concrete
 - Masonry pavers over concrete base
 - Any similarly durable materials that meet the City's sidewalk standards

STREET TREE SPECIES

Best	Good
<i>Small Canopy</i> <i>(Min. 15' spacing)</i>	Ginkgo (<i>Med.</i>)
Redbud	Pecan (<i>Large</i>)
Mexican Plum	Prohibited
Eve's Necklace	Hackberry
<i>Medium Canopy</i> <i>(Min. 25' spacing)</i>	Sycamore
Caddo Maple	Silver Maple
Bald Cypress	Mulberry
<i>Large Canopy</i> <i>(Min. 35' spacing)</i>	Siberian Elm
Texas Red Oak	Mimosa
Shumard Oak	Arizona Ash
Live Oak	Cottonwood
Durand Oak	Willow
Lacebark Elm	Bradford Pear
Cedar Elm	Chinese Pistache

ROADSIDE ZONES



*Street tree/
furniture zone*

- Trees
- Lights
- Bike Racks
- Tables

Pedestrian way

- Clear walkway
- Awnings and other shade elements

5.B. Streets and Other Public Spaces (cont.)

3. STREET LANDSCAPING AND FURNISHINGS (cont.)

c. Pedestrian Lights

1. New public and private development shall provide pedestrian lights within the roadside's street tree/furniture zone.
2. Pedestrian lights shall be located approximately at the midpoint between two trees and spaced a maximum of 60 feet apart. Where necessary, spacing exceptions may be made to accommodate mature trees, curb cuts, fire hydrants and other infrastructure elements.
3. Guidelines
 - Continuity of lighting style throughout a neighborhood is encouraged.
 - Lights should be durable and easy to maintain or replace.

d. Guidelines for other roadside elements

1. Bike racks, trash bins, and seating should be incorporated into streetscape designs on "Main Streets" and other streets with high levels of pedestrian activity. Continuity of style throughout a neighborhood is encouraged. These elements should be durable, cost effective, and easy to maintain.
2. Transit shelters should:
 - Be located in areas with a high level of pedestrian activity, but shall not impede pedestrian traffic or wheelchair circulation.
 - Provide comfortable seating and should not use the standard, stand-alone advertising benches seen at many bus stops.
 - Where feasible, incorporate other streetscape elements, such as water fountains, newspaper boxes, trash bins, and/or signage.
 - Reflect design excellence and should be designed as more than simple utilitarian fixtures. Creative and timeless designs that complement other streetscape furnishings are encouraged.
 - Be constructed of durable, attractive, and sustainable materials that are easy to maintain.

PEDESTRIAN LIGHTS



Since its inception, Fort Worth South, Inc. has worked with developers to install pedestrian lights as part of new construction projects, including this development along College Avenue. The roadside standards in this section promote the installation of lights throughout the Near Southside.

BIKE RACKS



Bike racks that are easy to recognize, attractive, and simple to use help make the Near Southside bike-friendly, providing convenience and security.

5.B. Streets and Other Public Spaces (cont.)

4. PUBLIC PARKS AND PLAZAS

- a. **Park Dedication Policy** – The City's park dedication policy applies to new residential units.
- b. **Plazas** – Plazas are urban public spaces that are more formal than parks and have a higher degree of hard surfaces and pedestrian traffic. Guideline for impervious surface area: Maximum 40 percent.
- c. **Parks** – Parks are open spaces that have less hard surface and pedestrian traffic than plazas. Parks typically include both active and passive recreation areas. Guideline for impervious surface area: Maximum 20 percent.
- d. **Hours** – Publicly and privately maintained public spaces shall be publicly accessible during daylight hours. Access may be limited at other times.
- e. **Design guidelines** – Publicly and privately developed public spaces should conform to the following guidelines, to the extent possible.
 1. Design all public spaces other than large recreational parks to be human-scale and visually interesting.
 2. Locate and design public spaces so that they are clearly visible and easily accessible.
 3. Incorporate space-defining and active edges – such as multi-story façades with ground floor restaurants – but avoid blank walls. Cafés and shops that attract pedestrians are ideal edge uses.
 4. Plan for prevailing sun angles and climatic conditions.
 5. Avoid locating public spaces on corner lots. Corners are better suited for buildings that establish a strong urban street edge.
 6. Avoid sunken plazas and architectural bench arrangements; these often fail to promote use.
 7. Provide flexible space for programmed uses, but design such spaces so that they do not appear barren when there is no programming.
 8. Incorporate features that attract users, such as fountains, public display areas, or interactive sculpture.
 9. Incorporate landscaping to provide natural shade and/or to soften hardscape areas.

5. PUBLIC ART GUIDELINES

- a. **Public art in private projects** – Where feasible, developers are encouraged to integrate art into the design process for buildings and public spaces.
- b. **Public art in public projects** – Public agencies are encouraged to integrate art in the design of all publicly visible infrastructure, including retaining walls and public spaces.
- c. **Fort Worth Art Commission assistance** – The Fort Worth Art Commission may assist with identifying appropriate artists and/or advise on selection processes for projects.

PUBLIC SPACES



John Hancock Plaza, Chicago
Source: Project for Public Spaces

The standards and guidelines encourage a variety of public spaces on the Near Southside, ranging from the type of active urban plaza shown above to more passive and heavily landscaped neighborhood parks.

PUBLIC ART



Eric McGehearty's
United We Stand
at Fire Station #8

The firefighter boots sculpture at Fire Station #8 was funded through the City's public art program. In addition to art that is integrated into City projects, privately sponsored public art can greatly enhance a development project's appeal and value.

5.C. Building Location and Orientation

1. BASE SETBACK STANDARDS

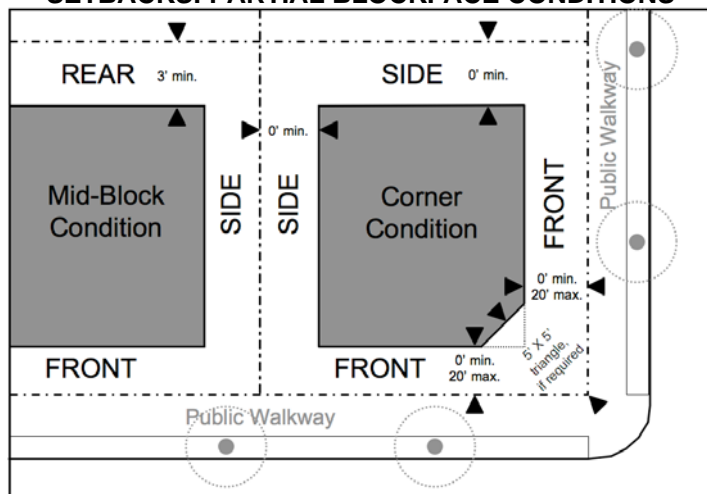
New buildings shall be located in relation to the boundaries of their lots according to the setback standards listed below and depicted to the right.

Front Setback	0 ft. min., 20 ft. max. except as described in Section 5.C.2. below.
Side Setback	0 ft. min., except as described in Section 5.C.2.e. below.
Rear Setback	3 ft. min.
Corner Setback	5' x 5' dedication when required by Section 5.C.2.f. below.

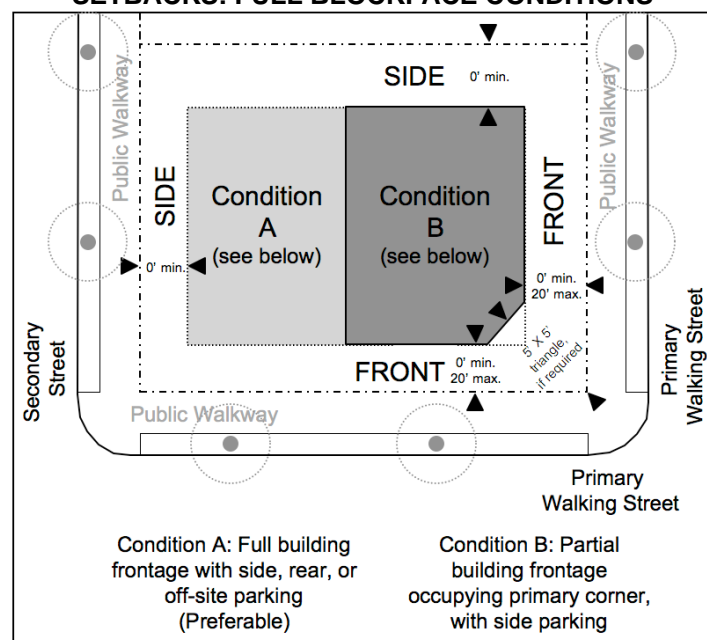
2. CONTEXTUAL SETBACK STANDARDS

- Buildings fronting designated “Main” streets** – Front setback is 0 ft. min. and 10 ft. max. (Street classification maps shown in Section 4.C.)
- Buildings fronting public spaces** – Buildings that front pocket parks, plazas, or other public spaces may exceed the maximum front setback.
- Oleander Walk T4-N zone** – Front setback is 5 ft. min. and 10 ft. max. along Oleander Walk; 0 ft. min. and 10 ft. max. along other streets.
- “On-street” parking or public walkways located on private property** – If angled, perpendicular, or parallel parking is located on private property but functions the same as on-street parking, the front setback is measured from the edge of the public walkway. Similarly, if a required public walkway (see Section 5.B.2) encroaches onto private property, the setback is measured from the walkway edge as shown in diagrams to right.
- Side yards adjacent to existing buildings** – When an abutting property with an existing building has windows facing to the side, any new building shall provide at least 10 feet of separation.
- Corner buildings** – For corner buildings at street and alley intersections that do not include all-way stop signs or traffic signals, a triangular dedication measuring 5' by 5' is required by the subdivision ordinance. The triangle shall be measured from the property line and clear of visibility obstructions.
- Interior buildings** – Interior buildings may be constructed if the project also includes frontage buildings that are oriented to public streets. Specifically, buildings may exceed the setback if at least 60 percent of the public street frontage on each block face within the development contains buildings within the maximum setback of 20 ft. The frontage percentage is reduced to 50 percent for institutional or industrial campuses in “I” zones.

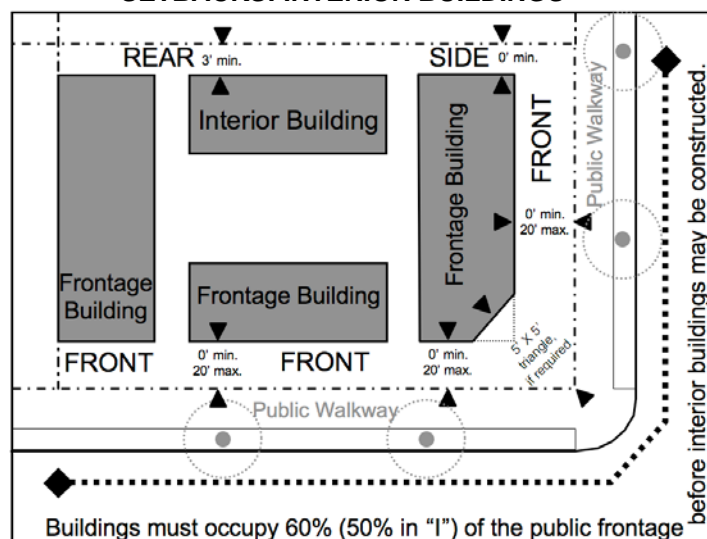
SETBACKS: PARTIAL BLOCKFACE CONDITIONS



SETBACKS: FULL BLOCKFACE CONDITIONS



SETBACKS: INTERIOR BUILDINGS



5.C. Building Location and Orientation (cont.)

3. PEDESTRIAN ENTRANCES

- a. **Primary pedestrian building entrances** shall be located on the street frontage of the building. For buildings fronting other public spaces, the primary pedestrian entrance shall be oriented to and accessible from the public space.
- b. **Individual retail entrances** – Each retail use with exterior ground level exposure along a street or public space shall have an individual public entry from the street or public space.
- c. **Corner building guideline** – Entrances to corner buildings with ground floor retail uses should be located at the corner.

4. FENCING

- a. **Front yard perimeter fencing prohibited** – Conventional gated complexes with perimeter security fencing along public streets are prohibited. Specifically, exterior security fences and gates that are located along public streets, along private streets or walkways that are publicly accessible through a public use easement, or along publicly accessible open space shall not extend beyond building facades; i.e., these fences shall not be located in the area between building facades and the property line.
- b. **Privacy fences** – A front yard fence or railing not exceeding 4 feet in height may extend beyond building façades if the fence encloses a private patio, yard, or sidewalk eating area. The fence is not permitted to encroach on a required pedestrian walkway (see Section 5.B.3.b.).

5. DRIVE-THROUGH DESIGN STANDARDS

- a. **Guideline** – Conventional drive-through uses do not support a pedestrian-oriented environment and are inconsistent with the urban redevelopment goals for the Near Southside. Although drive-through facilities are discouraged, the following standards shall apply to facilities approved by the UDC.
- b. **Windows and stacking lanes** – Drive-through windows and stacking lanes shall not be located within the front yard setback or along facades that face a street; where possible, they shall be located to the rear of buildings.
- c. **Circulation** – The design and location of the facility shall not impede vehicular traffic flow and shall not impede pedestrian movement and safety. Driveways shall not be located on Main streets, such as Magnolia Avenue and South Main Street (see classification maps in Section 4.C.). Shared driveways and/or driveways located off of non-arterial streets should be used, where possible.
- d. **Screening** – Architectural elements, landscaping, and/or other screening elements shall be used to minimize the visual impacts of the drive-through facility.

PEDESTRIAN ENTRANCES



Pedestrian-oriented projects orient primary entrances to public sidewalks, and corner retail/service buildings often include corner entrances.

FENCING



Conventional gated complexes are not allowed, but short privacy fences that clearly delineate private yards and patios are compatible.

URBAN DRIVE-THROUGHS



The well-screened Wells Fargo drive-through facility is a good example of a compatible design that fits into a walkable environment.

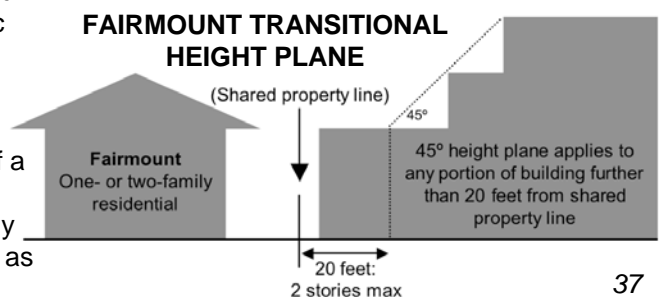
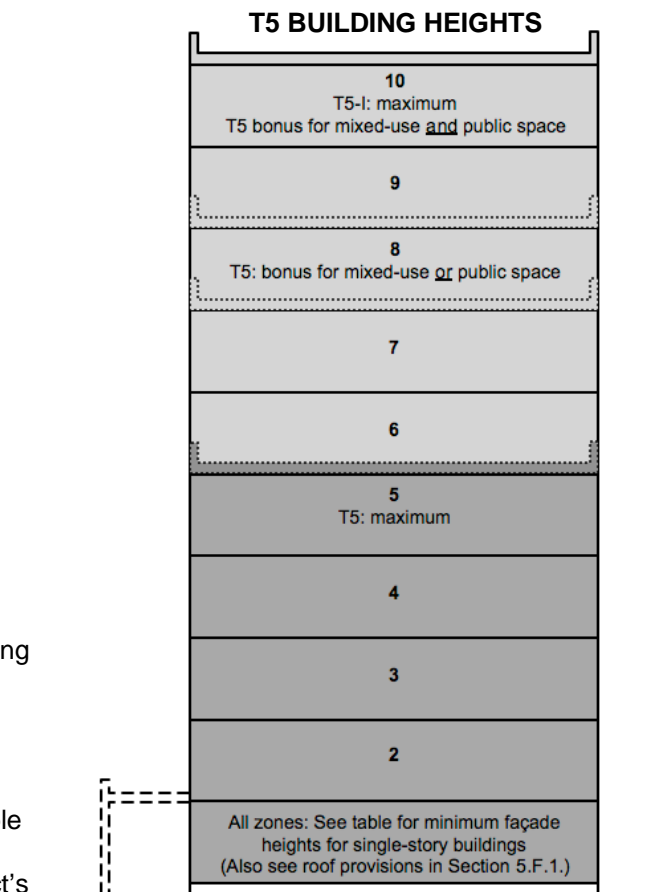
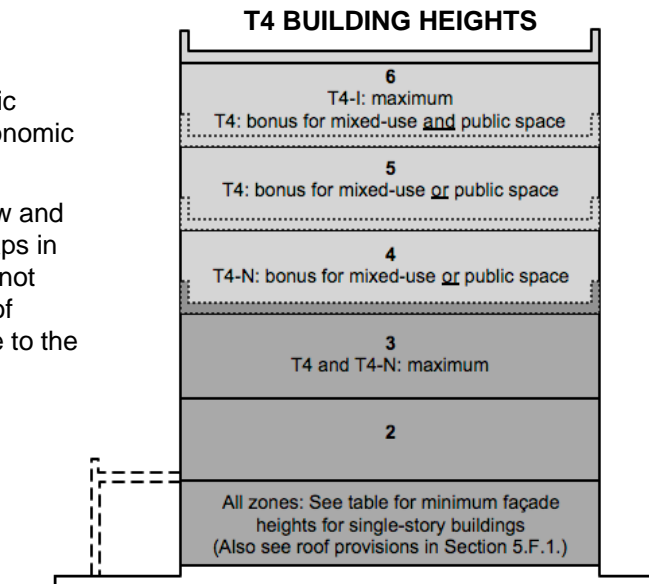
5.D. Building Height

1. HEIGHT GUIDELINE – New single-story buildings are strongly discouraged. Multi-story buildings are consistent with the historic urban character of the district and are essential in achieving economic development and urban design goals for the Near Southside.
2. BUILDING HEIGHTS shall conform to the standards listed below and shown in the diagrams to the right. Zones are shown on the maps in Section 4.B. Building height is measured by number of stories, not including a raised basement or inhabited attic; i.e., the number of complete stories between the average grade of the frontage line to the eave of a pitched roof or to the surface of a flat roof.

Minimum heights (Also see 5.D.1. guideline above)	New façades along public streets and public spaces: <ul style="list-style-type: none"> • 15 ft. for buildings < 4,000 square feet • 18 ft. for buildings ≥ 4,000 square feet Ground floors of multistory non-residential buildings: min. 10 ft., floor to ceiling.	
Maximum heights	T4 and T4-N	3 stories
	T4-I	6 stories
	T5	5 stories
	T5-I	10 stories
Maximum heights with a) mix of use and/or b) public space bonuses (See Section 5.D.3.)	T4-N: a <u>or</u> b	4 stories
	T4: a <u>or</u> b	5 stories
	T4: a <u>and</u> b	6 stories
	T5: a <u>or</u> b	8 stories
	T5: a <u>and</u> b	10 stories

3. HEIGHT BONUS REQUIREMENTS

- a. **Mix of residential and non-residential uses** – The building must include at least 20% residential and 10% office, restaurant, and/or retail uses, as measured by gross floor area.
 - b. **Public space** – The project must include a publicly accessible, privately maintained park, plaza, or other usable outdoor public space. Public access must be maintained through a public access easement. The ratio of the project's gross floor area (including height bonus area) to the public space's surface area shall not exceed 10:1, and no public space shall be less than 2,500 sq. ft. (See Sec. 5.B.4.)
4. FAIRMOUNT TRANSITIONAL HEIGHT PLANE – Applies to properties that share a property line with a one- or two-family house located in the Fairmount Historic District. Any portion of a building within 20 feet of the property line shall not exceed 2 stories. A 45 degree transitional height plane shall apply to any portion of a building further than 20 feet from the property line, as depicted in the diagram to the right.



5.E. Parking and Driveways

1. OFF-STREET PARKING REQUIREMENT – None, except for properties located within 250 feet of a one- or two-family zoning district. For those properties, the requirements in Section 6.201 of the Zoning Ordinance, reduced by 25%, shall apply. Uses within historically significant buildings are exempt.
GUIDELINE – Shared parking garages are encouraged.

2. SURFACE PARKING CAP – The number of off-street spaces shall not exceed 100% of the total prescribed by Section 6.201 of the Zoning Ordinance, unless a parking study demonstrates need for additional spaces.

3. SURFACE PARKING LOTS – Lots shall be located behind or to the side of buildings as depicted in the diagrams to the right. (Parking lot design standards are contained in Section 6.201 of the Zoning Ordinance.)
GUIDELINE – Surface parking lots that front streets are discouraged; all site plan options that minimize surface lots along public streets should be explored.

4. SURFACE PARKING SCREENING – Parking lots that front a street shall be separated from the sidewalk by a decorative and durable screen at least 4 ft. in height.
GUIDELINE – Screening walls attached to buildings should be designed as architectural extensions of the building, constructed of the same materials and style.

5. SECURITY FOR PARKING LOTS– A fence not exceeding 6 ft. in height may be used to secure driveways and parking lot entrances if it is not located in the front yard. Chain link and barbed wire fences are not allowed.

6. RESIDENTIAL GARAGES – Garages should not front public pedestrian space. Garages shall be accessed from alleys or rear driveways or shall be detached and located at the rear of the site.

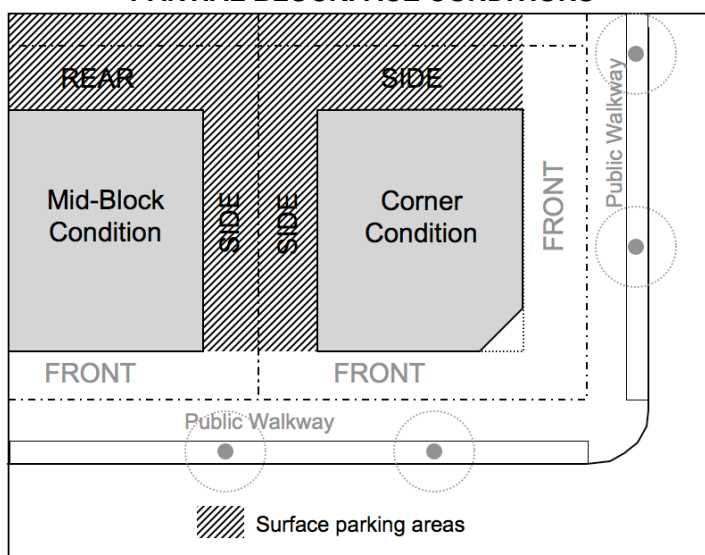
7. COMMERCIAL OR PUBLIC PARKING GARAGES shall be located at the interior of a block or underground and shall not be visible from public rights-of-way, unless the UDC approves an exception. Exception requirement: Ground floor commercial uses, or space adaptable for future commercial use, shall be integrated into parking garages located along public streets.

8. DRIVEWAY LOCATION – Private vehicular driveways shall not be located along “Main” streets. This provision applies as a guideline in “I” zones.

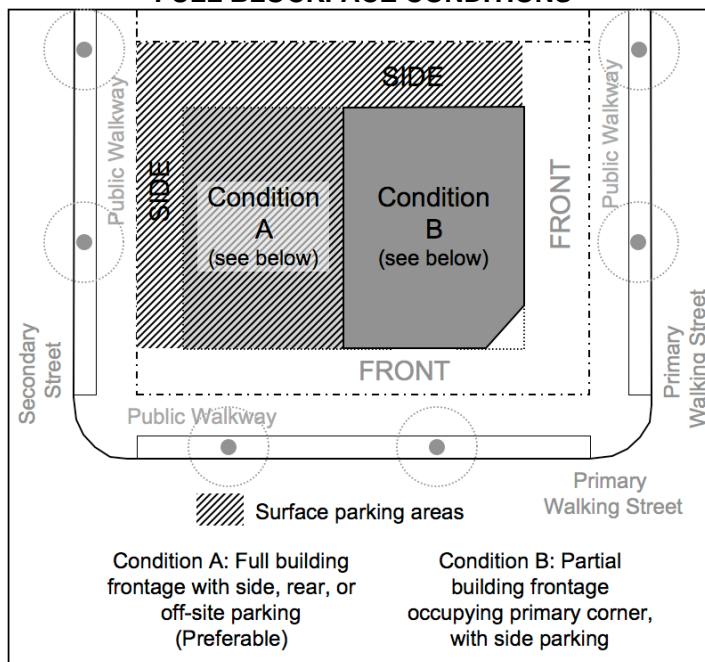
9. DROP-OFF AND LOADING AREAS – On-street drop-off and loading areas support a pedestrian-oriented district and may be approved by the City’s Traffic Engineer. Off-street loading and drop-off areas that are located between the building and the sidewalk are allowed in T4-I and T5-I zones if a continuous sidewalk is provided adjacent to the street.

GUIDELINE – Drop-off and loading areas proposed in zones other than T4-I and T5-I should be located under the overhang of a building that maintains the urban street edge within the maximum front setback.

SURFACE PARKING: PARTIAL BLOCKFACE CONDITIONS



SURFACE PARKING: FULL BLOCKFACE CONDITIONS



COMPATIBLE PARKING FACILITIES



The parking standards are intended to minimize the impact of parking facilities.

5.F. Architectural Standards

1. ROOFS

- Single-story buildings** – New single-story buildings shall not have large expanses of sloped roofs. Roof slope shall not be greater than 1:12. Sloped roof elements that are not part of the actual roof, similar to elements found on certain historic commercial buildings, are permitted.
- Roof design guidelines** –
 - Parapet roofs or low-sloped roofs are encouraged.
 - Visually dominant pitched roofs on multi-story buildings are strongly discouraged, but partial pitched roofs or pitched roof elements are generally compatible.
 - “Green” roofs that utilize plants to absorb rainwater and reduce ambient air temperatures are strongly encouraged.
- Screening of rooftop equipment** –Parapets or other screening elements of sufficient height shall conceal mechanical equipment from street-level views.

ROOFS ON SINGLE-STORY BUILDINGS



PEDESTRIAN PROTECTION



- PARALLEL FRONTAGES (Guideline)** – Building facades should be built parallel to the street frontage, except for chamfered corners.
- ENTRANCES** facing a pedestrian way shall incorporate elements that protect pedestrians from the sun and rain.
- AWNINGS, GALLERIES, ARCADES, AND BALCONIES (Guidelines)** – Awnings, galleries, and arcades are encouraged for all ground floor retail uses so as to provide pedestrian protection and visual interest. Where feasible, balconies are encouraged and should be designed as an integral part of the building.
- FAÇADES** – The following standards apply to building facades facing a publicly accessible street or other public space (except alleys).

ENTRANCES FROM PEDESTRIAN WAYS



- Façade variation** – Buildings shall, at a minimum, incorporate:
 - Expression of structural elements, such as:
 - Floors
 - Vertical support
 - Foundation
 - Façade articulation through the use of projecting and recessed elements.
 - Variety in materials, material pattern, or color.

FAÇADE VARIATION



(Façade section continued on next page.)

5.F. Architectural Standards (cont.)

b. Building materials

- 1) New building façades facing public areas (except alleys) shall conform to the material standards listed to the right. The lists could evolve as technology changes. Creative design is encouraged, and projects that incorporate unlisted materials may be submitted for staff and UDC review.
- 2) Leadership in Energy and Environmental Design (LEED) Buildings – Buildings following the U.S. Green Building Council's LEED certification requirements shall be exempt from building material standards.

- ### c. Fenestration (all buildings)
- New building façades fronting on publicly accessible streets or other public spaces (except alleys) shall have openings and transparent (not mirrored) glazing that together constitute not less than 25 percent of the façade. This provision applies as a guideline in “I” zones.

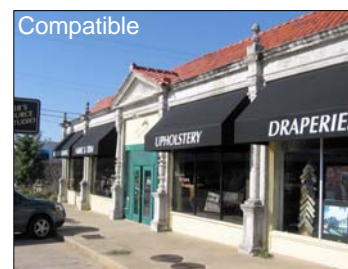
d. Ground floor transparency for non-residential uses

- 1) For all ground floor nonresidential uses located along publicly accessible streets and other public spaces, at least 40 percent of the wall area between 2 and 12 feet shall consist of glazing (i.e., clear windows or doors). This provision applies as a guideline in “I” zones.
- 2) Guideline – Ground floor restaurants, cafés, and bars are encouraged along “Main” streets and should have windows and doors that can be opened to provide direct access to the sidewalk when weather permits (i.e., when interior heating or cooling systems are not necessary.)

- ### e. Lower level delineation for nonresidential or mixed-use buildings
- Façades oriented to a publicly accessible street or other public space shall include clear delineation between the first or second level and the upper levels with a cornice, canopy, balcony, arcade, or other architectural feature.

- ### f. Parking garage façades on “Main” Streets
- Parking garages shall not have exposed structured parking at the ground floor level along a “Main” street.

MATERIALS ALONG PUBLIC STREETS



BUILDING MATERIALS

Primary Materials

- Brick
- Stone, or stone veneer with cavity wall construction
- Stucco
- Glass curtain wall system
- Metal Panels – individual or curtain wall systems
- Concrete – finish should be to an architectural level
- Wood
- Cement composite board (not simulating wood)
- Tile – terra cotta, porcelain, or ceramic

Limited Accent and Trim Materials

- All primary materials listed above
- Metal – galvanized, painted, or ornamental
- Pre-cast masonry (trim and cornice only)
- EIFS (Exterior insulation and finish system)
- Concrete fiber simulated wood siding

Inappropriate Materials

- Applied stone without cavity wall construction
- Vinyl or aluminum siding
- Mirrored glass

GARAGE FAÇADES ON “MAIN” STREETS



- ## 6. ARCHITECTURAL VARIETY IN LARGE PROJECTS
- Each sequential block of new construction shall contain unique building façades so as to encourage architectural variety within large projects. While the use of similar architectural elements on multiple buildings is acceptable; large, visually monotonous projects that repeat the same exterior buildings design(s) and do not contribute to visual diversity and human-scale character are inappropriate.

7. OUTDOOR STORAGE OR DISPLAY

- a. **T4, T4-N, and T5 zones** – Development in these zones shall comply with the standards for outdoor storage and display set forth in Section 5.306 of the Zoning Ordinance. The UDC may consider exceptions if outdoor materials are sufficiently screened by decorative fencing or other screening elements.
- b. **“I” zones** – Decorative fencing or other screening elements shall be used to minimize the visual impact of outdoor storage adjacent to public streets or other public spaces.

7. SIGNS –

a. **Types**

- 1) On premise attached and monument style signs are permitted.
- 2) Pole signs are not permitted.
- 3) Off premise signs are not permitted.

b. **Basic Sign Standards** – Signs shall conform to the basic dimensional regulations set forth in Chapter 6, Article 4 of the Zoning Ordinance.

c. **Design Guidelines** – Signs should conform to the design guidelines contained in Appendix A, excerpted from the Downtown Sign Standards. The following are general guidelines for sign design:

- 1) Location: Signs should be positioned to emphasize or accent building elements such as storefront openings or entrances.
- 2) Alignment: Signs should be aligned with those on neighboring buildings to promote visual order on the block, to avoid visual clutter, and to enhance legibility.
- 3) Dimensions and Scale: Signs should be consistent with the human scale of Near Southside buildings and blocks.
- 4) Number of Signs: The number of signs per building façade should be limited to the fewest necessary to clearly identify businesses located within the building.
- 5) Colors and Materials: Signs should relate in color and material with the building facade and streetscape. Colors should contribute to legibility and visual appeal.
- 6) Graphics: Sign graphics should be simple. Symbols and logos on signs are quickly read and easily remembered. Lettering should be in proportion to the size of the sign.
- 7) Lighting: Indirect illumination is recommended. Lighting sources should be external, shielded, and directed only at the sign. Internal illumination is acceptable when the letters themselves – not the background – are lit.

SIGNS



The signs above are all consistent with the design guidelines contained in Appendix A.

5.G. On-Site Landscaping (Also see Section 5.B.3.)

1. **TREE PRESERVATION AND CANOPY REQUIREMENTS** – The following provisions of the Tree Ordinance apply:

a. **Preservation Of Significant Trees** – Significant trees at least 30 inches in diameter (94.25 inches in circumference) can only be removed by permit of the City Forester. Criteria for the permit will be one of the following conditions:

- 1) Retention of other existing trees on the same site with a canopy area 1 ½ times the area of the specific tree's canopy, or
- 2) Removal will be granted if planting of new trees at five (5) times greater in canopy area than the removed specific tree canopy (refer to list of canopy trees on the following page), or
- 3) Payment into the tree fund based upon the total diameter of the specific tree times \$200 per diameter inch, or
- 4) Urban Forestry Board approved plan that mitigates the removal of the large tree.

Replacement of any tree preserved and that dies within five years due to construction or development activities will be the responsibility of the original applicant. Replacement will be new trees with a minimum of 3 inches each in diameter and equal to five times the lost canopy. Tree replacement will be guaranteed for a period of two years.

b. **Trees In Surface Parking Lots** – All new construction projects shall provide at least 40% tree canopy coverage of surface parking lots (refer to the categorized list of canopy trees on the following page).

c. **Tree Protections and Warranty/Replacement** – The provisions of Zoning Ordinance Chapter 6, Article 3, Section 6.301.K.6. apply. These provisions ensure that sufficient care is taken during construction to protect existing trees, and that any preserved or newly planted tree required by the provisions of Sections 5.G.1. and 5.B.3. In this document shall be replaced if it dies within 5 years.

2. FRONT YARD LANDSCAPING GUIDELINE – Although no front yard setback is required, where there is a setback, front yard landscaping in addition to required trees should be provided and maintained in areas outside of ground level elements such as patios or porches. Native and drought tolerant species should be used, and irrigation systems should be designed to minimize water demand.

CANOPY TREES APPROVED BY THE CITY FORESTER (Other species considered on case-by-case basis.)

Large Canopy Trees (2,000 square feet)

Pecan #	Carya illinoensis
Deodar Cedar	Cedrus deodara
Green Ash	Fraxinus pennsylvanica
Southern Magnolia#	Magnolia grandiflora
Bur Oak*	Quercus macrocarpa
Chinquapin Oak	Quercus muhlenbergii
Shumard Oak #	Quercus shumardii
Texas Red Oak	Quercus buckleyi
Live Oak *	Quercus virginiana
American Elm	Ulmus Americana
Cedar Elm *	Ulmus crassifolia
Lacebark Elm	Ulmus parvifolia

Medium Canopy Trees (700 square feet)

Caddo Maple *	Acer barbatum var. Caddo
Bigtooth Maple *	Acer grandidentatum
Common Persimmon	Diospyros virginiana
Texas Ash	Fraxinus texensis
Ginkgo	Ginkgo biloba
Kentucky Coffeetree	Gymnocladus dioica
Eastern Red-Cedar *	Juniperus virginiana
Eldarica (Afghan) Pine *	Pinus eldarica
Italian Stone Pine	Pinus pinea
Honey Mesquite *	Prosopis glandulosa
Blackjack Oak *	Quercus marilandica
Monterrey (Mex white) Oak *	Quercus polymorpha
Pond Cypress	Taxodium ascendens
Bald Cypress *	Taxodium distichum

Small Canopy Trees (100 square feet)

Japanese Maple #	Acer palmatum
Common button-bush	Cephalanthus occidentalis
Redbud *	Cercis Canadensis
Desert Willow *	Chilopsis linearis
Rough-leaf Dogwood #	Cornus drummondii
Texas Persimmon *	Diospyros texana
Carolina buckthorn #	Frangula caroliniana
Yaupon Holly *	Ilex vomitoria
Deciduous Holly	Ilex deciduas
Crepe Myrtle *	Lagerstroemia incisa
Mexican Plum *	Prunus mexicana
White Sin Oak *	Quercus sinuata var. breviloba
Flameleaf Sumac *	Rhus lanceolata
Eve's Necklace *	Sophora affinis
Mexican buckeye *	Ugnadia speciosa
Rusty Blackhaw	Viburnum rufidulum

* Drought tolerant species

Not recommended for parking lots or high heat areas



Tree preservation standards promote the retention of significant trees like these along 8th Avenue.



Trees planted in parking lots, together with pervious surfaces such as permeable concrete, improve environmental quality and help reduce stormwater runoff.

6.A. Mix of Use Requirement for Large Developments

The following standards are intended to ensure that large projects in mixed-use zoning districts include a mix of uses, or contribute to the creation of a larger mixed-use area. These standards do not apply in "I" zones.

1. **CONCEPTUAL LAND USE PLAN** – Developers of projects equal to or larger than 3 acres in size shall submit a conceptual land use plan for approval by the Planning Director. The conceptual land use plan must be approved before a building permit application is accepted. The Planning Director may require a conceptual land use plan for a project smaller than 3 acres if it is part of a project larger than 3 acres.

The conceptual land use plan shall illustrate the location and calculated land area of land uses on the site, using the following land use categories (see Section 6.C. for detailed list of land uses within these categories; exceptions noted below):

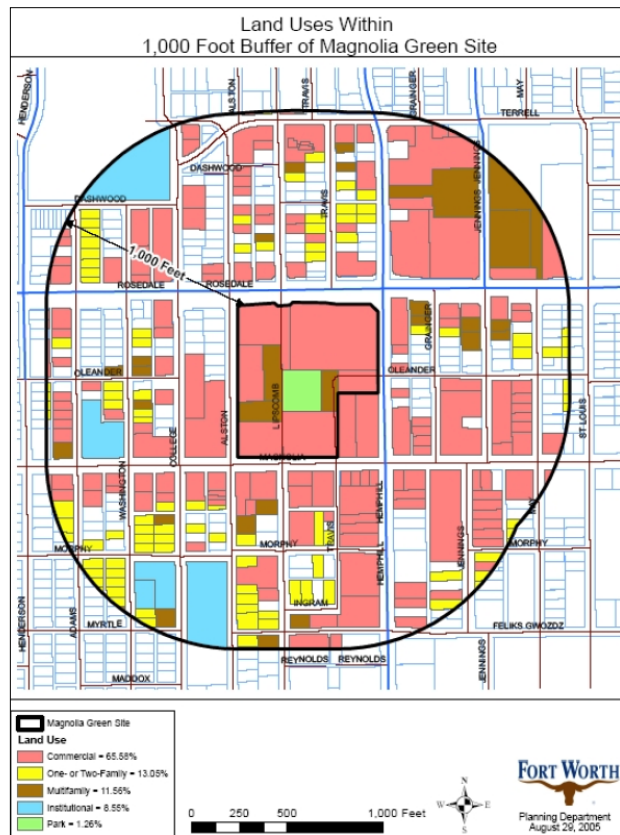
- One- or two-family residential
 - Multifamily residential
 - Industrial and commercial (including medical offices)
 - Public and civic uses (excluding parks and medical offices)
 - Mixed-use buildings (must include at least 20% residential and 10% non-residential)
 - Publicly accessible parks and plazas
- Parking facilities and private open spaces shall be classified the same as the primary land use they serve.

2. **PROJECT TEST** – The conceptual land use plan shall be approved if it shows that:
 - a. The project includes uses within at least two of the land use categories, and
 - b. No land use category other than mixed-use buildings occupies greater than 2/3 of the total land area.

If a project does not comply with the project test, then the vicinity test in Section 3. shall apply.

3. **VICINITY TEST** – Developments not complying with the project test are permitted if:
 - a. The Planning and Development Director determines that the following conditions are satisfied:
 - 1) The proposed land use at any location within the proposed development site must be within a walking distance of 1,000 feet of a different land use, as measured by the shortest pedestrian route, and
 - 2) The percentage of any single land use category other than mixed-use buildings within a 1,000-foot radius of any location within the proposed development site shall not be greater than 2/3 of the total land area within the radius. The proposed development shall be included in the calculation of this percentage. Undeveloped or agricultural property located within the radius shall not be included in the calculation; or
 - b. The Director determines that the developer has demonstrated that unique site conditions (e.g. adjacency to natural features, highways, freight yards, etc.) make compliance with the conditions of section a. above impractical in certain areas of the development site.

MIX OF USE TESTS



Projects of at least 3 acres must provide a mix of uses that conforms to the "project test" requirements of Section 2. or must contribute to the creation of a larger mixed-use area. City staff conducts a "vicinity test" to make this determination for large single-use projects.

6.B. Residential Requirement in “N” Neighborhood Zones

The following standard is intended to enhance the residential character of existing neighborhoods and to promote the development of additional housing in those areas.

1. RESIDENTIAL REQUIREMENT – Single-use buildings of commercial, retail, or other non-residential character are prohibited in “N” neighborhood zones. (See development zone maps in Section 4.B.). Buildings within these zones must include residential uses. For this section only, bed and breakfast uses are considered residential. Buildings that incorporate a mix of residential and non-residential uses are allowed and encouraged (see height bonus for mix of uses in Section 5.C.)



Mixed-use projects that incorporate housing on upper floors are appropriate in “N” zones.

6.C. Permitted Land Uses

The following table lists land uses permitted in the NS District. A (P) indicates that these uses are allowed by right in the applicable development zone. A (SE) indicates that these uses are allowed by special exception from the Board of Adjustment. An asterisk (*) indicates that the uses are allowed but are subject to supplemental development standards. Please refer to the the Zoning Ordinance for all use definitions and any referenced supplemental standards.

		NS-T4	NS-T5	Supplemental Standards
RESIDENTIAL USES				
Household Living	One-family detached dwelling	P	P	
	One-family zero lot line detached	P	P	
	One-family zero lot line attached (twin home)	P	P	
	Two one-family detached	P	P	
	Duplex/two-family attached dwelling	P	P	
	One-family attached (townhouse, rowhouse)	P	P	
	Cluster housing	P	P	
	HUD-code manufactured housing			
	Industrialized housing	P*	P*	5.116B
	Manufactured home park			
	Manufactured home subdivision			
	Mobile home			
	Multifamily dwelling (apartment)	P	P	
	Boarding or lodging house			
	Community home	P*	P*	5.110
Group Living	Fraternity or sorority house	P	P	
	Group home I	P*	P*	5.115
	Group home II	P*	P*	5.115
	Halfway house			
	Shelter			
	One dwelling unit when part of a business	P	P	

6.C. Permitted Land Uses (cont.)

PUBLIC AND CIVIC USES				
Education	College or University	P	P	
	Day care center (child or adult)	P	P	
	Kindergarten	P	P	
	School, elementary or secondary (public or private)	P	P	
Government	Animal Shelter			
	Correctional facility			
	Government maintenance facility			
	Government office facility	P	P	
	Museum, library or fine art center	P	P	
	Probation or parole office		P*	5.127
Healthcare Facilities	Ambulance dispatch station	P	P	
	Assisted living facility	P	P	
	Blood bank	P	P	
	Care facility	P	P	
Healthcare Facilities	Health services facility; including doctor's office or medical clinic	P	P	
	Hospice	P	P	
	Hospital	P	P	
	Massage Therapy/Spa	P	P	
	Nursing home (with full medical services)	P	P	
Recreation	Center, community recreation or welfare	P	P	
	Center, community recreation or welfare, private or non-profit	P	P	
	Country club (private)	P	P	
	Country club (public)			
	Golf course	P	P	
	Golf driving range		P	
	Park or playground (public or private)	P	P	
Religious	Place of worship	P	P	
	Place of worship auxiliary use	P	P	
Utilities	Central utility plant for institutional or industrial uses		P	
	Communication Tower			
	Electric power substation	SE	P	
	Natural gas compressor stations	SE	SE	
	Power plant or central station light			
	Stealth Telecommunication towers	P	P	
	Telecommunications antenna (on structure)	P*	P*	5.136
	Telecommunications tower	SE*	SE	5.137
	Utility transmission or distribution facility	P*	P*	5.14

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
PUBLIC AND CIVIC USES				
Utilities	Wastewater (sewage) treatment facility			
	Water supply, treatment or storage facility	SE	SE	
COMMERCIAL USES				
Entertainment and Eating	Amusement, indoor	P	P	
	Amusement, outdoor			
	Baseball/softball facility (commercial)	P	P	
	Bowling Alley	P	P	
	Bar, tavern, cocktail lounge, club, private or teen	P	P	
	Circus			
	Club, commercial or business	P	P	
	Drive-in restaurant or business	P*	P*	5.112
	Gambling facility (including bingo)			
	Health or recreation club	P	P	
	Lodge or civic club	P	P	
	Massage Parlor	P	P	
	Massage Parlor/Bath		P	
	Museum/cultural facility	P	P	
	Racing; horse, dog or automotive			
	Restaurant, café, cafeteria	P	P	
	Sexually oriented business			
	Shooting or weapons firing range			
	Stable, commercial, riding, boarding or rodeo arena			
	Swimming pool, commercial	P*	P*	5.135
	Theater, drive-in			
	Theater, movie theater, or auditorium	P*	P	5.138
Lodging	Bed and breakfast inn	P*	P*	5.107
	Hotel, motel or inn		P	
	Recreational vehicle (RV) park			
Office	Bank, financial institution	P	P	
	Offices	P	P	
Retail Sales and Service	Antique shop	P	P	
	Appliance, sales, supply or repair	P*	P*	5.133
	Bakery	P	P	
	Barber or beauty shop	P	P	
	Boat rental or sales		P	
	book, stationary stores or newstand	P	P	
	Burglar alarms sales or service	P	P	

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
COMMERCIAL USES				
Retail Sales and Service	Business college or commercial school	P	P	
	Caterer or wedding service	P	P	
	Clothing/wearing apparel sales, new	P*	P*	5.133
	Clothing/wearing apparel sales, used	P*	P*	5.133
	Convenience store	P	P	
	Dance Studio	P	P	
	Dressmaking, custom; millinery shop	P	P	
	Duplicating Services	P	P	
	Feed store, no processing/milling	P	P	
	Firewood sales	P*	P	5.113
	Furniture sales, new and used (office & residential) in a building	P*	P*	5.133
	Furniture upholstery, refinishing or resale	P	P	
Retail Sales and Service	General merchandise store	P*	P*	5.133
	Greenhouse or plant nursery	P*	P*	5.114, 5.133
	Grocery store, meat market	P*	P*	5.133
	Gunsmithing, repairs or sales		P	
	Home improvement store	P*	P*	5.133
	Interior decorating	P	P	
	Kennel			5.117
	Large retail store	P*	P*	5.134
	Laundry or dry cleaning collection office	P	P	
	Laundry, dry cleaning or washateria	P	P	
	Leather goods shop	P	P	
	Liquor or package store	P	P	
	Locksmith	P	P	
	Marina, water taxi, recreation boat house			
	Medical supplies/equipment sales or rental	P	P	
	Mini-warehouses			5.143
	Mortuary or funeral home	P	P	
	Newspaper distribution center		P	5.124
	Optician	P	P	
	Pawn shop		P*	5.125, 5.133
	Pharmacy (drug store)	P	P	
	Photograph, portrait/camera shop or photo finishing	P	P	

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
COMMERCIAL USES				
Retail Sales and Service	Business college or commercial school	P	P	
	Caterer or wedding service	P	P	
	Clothing/wearing apparel sales, new	P*	P*	5.133
	Clothing/wearing apparel sales, used	P*	P*	5.133
	Convenience store	P	P	
	Dance Studio	P	P	
Retail Sales and Service	Recording studio	P	P	
	Retail sales, general	P*	P*	5.133
	Saddle or harness, repair or sales	P	P	
	Shoe shine shop	P	P	
	Studio, art or photography	P	P	
Retail Sales & Service	Tailor, clothing or apparel shop	P	P	
	Tattoo parlor		P	
	Taxidermist shop	P	P	
	Veterinary clinic w/indoor kennels	P*	P*	5.141
	Veterinary clinic w/outdoor kennels			5.142
Vehicle Sales & Service	Auto parts supply, retail	P	P	
	Automotive repair; paint and body shop	P	P	
	Car wash, full or self service		P*	5.108
	Gasoline sales	P	P	
	Mobile home or manufactured housing sales			
	Parking area or garage, storage commercial or auxiliary	P*	P*	6.202F
	Recreational vehicle (RV) sales & service		P	
	Service station		P	
	Truck stop w/fuel & accessory services			
	Vehicle junkyard			
	Vehicle sales or rental; including automobiles, motorcycles, boats or trailers	P	P	
	Vehicle steam cleaning		P	
INDUSTRIAL USES				
Light Industrial Services	Assaying		P*	5.103
	Assembly of pre-manufactured parts, except for vehicles, trailers, airplanes or mobile homes	P	P	
	Blacksmithing or wagon shop			
	Bottling works, milk or soft drinks		P	
	Carpet and rug cleaning		P	
	Chicken Battery or brooder			
	Coal, coke or wood yard			
	Crematorium		P	
	Electroplating		P	

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES				
Light Industrial Services	Fabricating or manufactured housing, temporary or office building			
	Food processing (no slaughtering)		P	
	Furniture or cabinet repair or construction	P	P	
	Furniture sales, w/outside storage/display (new/used)			
	Galvanizing, small utensils			
	Machine shops		P*	
	Manufacture of artificial flowers, ornaments, awnings, tents, bags, cleaning/polishing preparations, boats under 28' in length, brooms or brushes, buttons & novelties, canvas products, electronic equipment, clothing, suits, coats, or dresses for wholesale	P	P	
	Manufacture of aluminum, brass or other metals or from bone, paper, rubber, leather or plastics	P	P	
	Manufactured home/RV repair			
	Monument/marble works, finishing and carving only		P	
	Monument works, stone		P	
	Outdoor sales and storage	P	P	5.133
	Paint manufacturing, mixing or spraying		P	
Light Industrial Services	Manufacture of boxes other than paper		P	
	Pattern shop		P	
	Printing, lithographing, book-binding, newspapers or publishing		P	
	Rubber stamping, shearing/punching		P	
	Rubber stamp manufacture		P	
	Sheet metal shop		P*	5.131
	Warehouse or bulk storage		P*	5.143
	Welding shop, custom work (not structural)	P	P	
	Yards, contractor's, lumber or storage including vehicles		P	5.133
Heavy Industrial & Manufacturing				
	Animal by-products processing			
	Batch plant, concrete or asphalt (permanent)			

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES				
Heavy Industrial & Manufacturing	Brewery, distillery or winery		P	
	Brick, clay, glass, shale, tile or terra cotta products manufacture			
	Cement products plant			
	Cement, lime, gypsum or plaster of Paris manufacture			
	Cotton gin, cotton oil mill, bailing or compress			
	Creosote, treatment/manufacture			
	Egg cracking or processing			
	Furnace, blast; forge plant, boiler works manufacture			
	Galvanizing, sheet or structural shapes			
Heavy Industrial & Manufacturing	Gas (natural or artificial) manufacture processing/storage			
	Glue manufacture			
	Grain elevator			
	Magnesium casting, machining or fabricating			
	Manufacture of acetylene or oxygen gas, alcohol, adding machines, airplanes, automobiles, trucks & tractors (including assembly plants), ball or roller bearings, steel tanks, candles and celluloid, cash registers, cutlery, disinfectants, dextrin, dyestuff			
	Manufacture of basket material, bicycles, boots, boxes, caskets, shoes. Defined as other than paper.			
	Manufacture of dies, cores, die-casting molds			
	Manufacture, processing/production of hazardous chemicals			

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES				
Heavy Industrial & Manufacturing	Metal casting			
	Metal foundry plant or fabrication plant			
	Metal smelting, reclamation or ore reduction			
	Metal stamping, dyeing, shearing or punching			
	Mill, feed or flour			
	Mining quarry, dredging or excavation of rock, dirt, gravel, sand, stone			
	Packing plant			
	Paper or pulp manufacture			
	Petroleum refining or wholesale storage			
	Planing mill or woodworking shop			
	Poultry killing or dressing			
	Rock, cement crushers & stone quarry			
	Rolling mill			
	Soap manufacture			
	Soda or compound manufacture			
	Stoneyard, building stone, cutting, sawing or storage			
	Tar distillation/manufacturing			
	Tobacco (chewing) manufacture or treatment			
	Welding shop			
Transportation	Airport, aviation field, helistop or landing area	SE	SE	
	Passenger station	P	P	
Transportation	Railroad freight or classification yard			
	Railroad roundhouse or RR car repair shop		P	
	Railroad tracks: team, spur, loading or storage			
	Terminal; truck, freight, rail or water			
Waste Related	Landfill, recycling center, household hazardous waste or waste tire facility			

6.C. Permitted Land Uses (cont.)

		NS-T4	NS-T5	Supplemental Standards
INDUSTRIAL USES				
Waste Related	Pet cemetery			
	Recycling collection facility	SE*	P*	5.130
	Salvage yard (other than automotive)			
Wholesale	Wholesale: bakery, produce market or wholesale house		P	
	Wholesale office or sample room	P	P	
OTHER USES				
Agriculture	Agricultural uses			
	Stockyards or feeding pens (commercial)			
ACCESSORY USES				
Accessory Uses	Accessory use or building			5.302
	Satellite antenna (dish)	P*	P	5.304
	Stable, stockyards or feeding pens (non commercial)			
	Storage or display, outside	P*	P*	5.306
TEMPORARY USES				
Temporary	Amusement, outdoor (temporary)	SE*	SE*	5.400
	Batch plant, concrete or asphalt (temporary)			5.401
	Residence for security purposes (temporary)	SE*	SE*	5.404
	Trailer, portable; sales, construction or storage	P	P	
	Vendor, door to door	P*	P*	5.406
	Vendor, Food, Non-Potentially Hazardous Food	P*	P*	5.406
	Vendor, Food, Potentially Hazardous Food	P*	P*	5.406
	Vendor, Merchandise	P*	P*	5.406
	Vendor Transient			
	Vendor, Transient, Non-Potentially Hazardous Food	P*	P*	5.406
	Vendor, Transient, Potentially Hazardous Food	P*	P*	5.406

DEVELOPMENT REVIEW CHECKLIST	Complies?	Comments
Streets and Other Public Spaces		
Street standards (on-street parking, bike lanes, etc.)	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Roadside design (trees, walkways, pedestrian lights, etc.)	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Public park or plaza guidelines	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Public art guidelines	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Building Location and Orientation		
Base setbacks and contextual setbacks	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Primary pedestrian entrances from streets/public spaces	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Individual retail entrances from streets/public spaces	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Corner entrance for corner retail buildings	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
No front yard perimeter fence	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Privacy fence < 4 ft for residential patios or sidewalk eating	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Drive-through design standards	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Building Height		
Minimum height guideline: multistory buildings encouraged	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Base height limit for applicable development zone	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Height bonus for mix of use and/or public space	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Parking and Driveways		
Off-street parking for uses within 250' of one- or two-family	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Surface parking cap	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Surface parking located to rear or side of buildings	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Screening of surface parking	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Driveway and/or parking lot security fencing	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Residential garages accessed from alleys or rear driveways	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Commercial/public parking garages not visible from street	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
No new vehicular driveways on "Main" streets	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Drop-off and loading areas	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Architectural Standards		
No large expanses of sloped roofs on single-story buildings	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Parapet or low-slope roofs encouraged; visually dominant sloped roofs discouraged but pitched elements compatible	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Green roofs strongly encouraged	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Parapets screen rooftop equipment	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Frontages parallel to street, except chamfered corners	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Entrances have pedestrian protection	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Awnings, arcades, etc. encouraged for 1 st floor retail	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Balconies encouraged	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Façade variation	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Building materials	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Ground floor transparency for non-residential uses	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Fenestration at least 25% of all public facades	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Delineation between lower and upper floors for non-residential and mixed-use buildings	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Parking garage façade standards	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Architectural variety in large projects	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Outdoor storage or display	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
No pole signs	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Sign standards and guidelines	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
On-Site Landscaping		
Tree preservation and canopy standards	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Front yard landscaping	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Land Uses and Historically Significant Buildings		
Mix of use test for sites ≥ 3 acres	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Residential component for all projects in "N" zones	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
All land uses permitted per land use tables	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Supplemental standards, as applicable	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Historic buildings incorporated	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Required Drawings – Enclosed, as applicable:		
Site and landscape plans	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Building plans and elevations	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Material specifications	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Plans and specifications for proposed signs	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Description of proposed scope of work	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	
Photographs of site and existing conditions	<input type="checkbox"/> Yes <input type="checkbox"/> No <input type="checkbox"/> N/A	

APPENDIX - PUBLIC MEETING SUMMARIES

SOUTH MAIN URBAN VILLAGE COMMUNITY MEETING SUMMARY - JUNE 14, 2007

Hemphill Presbyterian Church 1701 Hemphill Street

Ann Kovich, with the TCB team, called the meeting to order and introduced members of the TCB team present for the kick-off meeting of the South Main Urban Village planning initiative. She also presented an overview of the unique projects for which TCB and EDAW have been responsible locally and nationally.

Ed Garza, with EDAW, discussed the importance of community input to the overall process. He defined an urban village as “an urbanized place with a mix of uses, jobs, public spaces, transportation, connections, pedestrian activity and a sense of place. Emphasizing why the City of Fort Worth created mixed-use zoning, he noted the benefits to be: (1) revitalization of central city commercial districts; (2) protection of single-family residential neighborhoods; (3) efficiency in the provision of public facilities and services; (4) convenience for residents and workers; (5) reduction in traffic congestion and support for transit; and (5) protection of the environment. The following are the consultant team’s assessment of the area’s strengths:

- Improvements to Lancaster Street and the transit station enhance access to the area.
- The area’s strong freeway traffic volume increases the potential for excellent freeway visibility.
- The area is in close proximity to multiple medical campuses, as well as a vibrant downtown.
- “Good bones” in many historic structures should respond well to streetscape enhancements, presenting opportunities for vertical mixed-use.
- Redevelopment activity at the Texas & Pacific Station, Magnolia Green, Homes @ Parker Commons, St. Joseph’s Gardens, and other locations reflects South Main’s potential.
- Success will be dependent on overcoming the poor reputation of the immediate neighborhood and filling the commercial vacancies.

Opportunities which Mr. Garza presented included an emphasis on “location, location, location,” such as the proximity to downtown, the West Medical District and the South Medical District. The area offers excellent opportunities for transit-oriented development, and there is space for adequate parking to support commercial and retail.

Constraints that were identified include the area’s deteriorating infrastructure, a low socio-economic market, homeless and transient populations, and businesses that are not reliant on the local market. Some persons commented on South Main’s “dark and dangerous” connection to downtown via the freeway underpass. Also there is a lack of vehicular traffic on South Main to support commercial revitalization.

Catalysts for redevelopment include the area’s connectivity to Dallas, its future connection to DFW Airport and the existing bus transfer center. Lighting improvements to the freeway underpass would be helpful. In addition, Fort Worth ISD’s properties totaling about 7.5 acres offer strong potential for redevelopment at the intersection of South Main and Pennsylvania.

After Mr. Garza’s presentation, attendees broke up into groups to further discuss the area’s positives and negatives, infrastructure needs and the benefits of mixed-use zoning. Once each group reached a consensus on these discussion topics, attendees came back together and through presentations by a representative of each group, the following consensus was determined.

- The area's most positive attributes are its proximity to downtown; its history and historic architecture; and its access to transit and potential for other transit uses.
- The area's most significant negatives are crime and the perception of crime; the lack of green space and parks; and the undesirable use of buildings as well as the vacancies, including the homeless population.
- The group was united in its preference of the brick streetscape as a vision for the future.
- Improvements most desired by the group included street beautification, a more pedestrian-friendly environment (including bike paths), affordable housing and general cleanup of trash and debris.
- The group supported mixed-use zoning applications.

SOUTH MAIN URBAN VILLAGE COMMUNITY MEETING SUMMARY - AUGUST 2, 2007

Broadway Baptist Church 305 W. Broadway

Ed Garza, with EDAW, called the meeting to order and introduced members of the TCB team present.

In presenting an overview of the meeting approach, Mr. Garza noted that the village has more than \$700,000 in grant dollars for a transportation or landscape project to be consistent with the plan now being developed. Based on discussions during the first public meeting of the South Main Village, he reiterated the community's perceptions of the area's strengths and weaknesses. Positive as ranked by participants were the area's historic fabric, transportation access, proximity to downtown, vacant land for development and the hospital district's workforce. Negatives as ranked by the participants were crime, vacant land, lack of services/retail/housing, no livable streetscapes, homeless and transit populations, and lack of green space. Top transportation/infrastructure improvement categories, as ranked by participants in the first public meeting, were street beautification, pedestrian improvements and vehicle access. Participants had preferred mixed-use zoning over conventional zoning examples.

Mr. Garza gave as best practices examples of mixed-use developments in three cities. He noted that Southtown in San Antonio, TX is a human scale urban village close to the CBD. Mixed-use zoning has provided for converted warehouse and industrial buildings, and new development including 500 apartment units and ground-level retail space. Pearl District in Portland, Oregon, is a former industrial area close to the CBD with access to multi-modal transportation. The urban success story boasts over 3,500 condos and apartments; more than 50 restaurants, a new Whole Foods grocery and a planned Super Safeway store. Frisco, Texas' Square and Sports Center has been created with millions of dollars to create an "old" Main Street. The pedestrian-friendly area emphasizes health and recreation. With youth soccer as an anchor, the area is an entertainment destination with a sports theme.

The South Main Village vision includes a health and medical culture; downtown sub-culture; transit-oriented development; Main Street USA replica; mixed-use density, mixed income residents; a warehouse district and urban square and parks. Mr. Garza pointed out that the vacancy rate in the core area is more than 50 percent, when it needs to be more than 90 percent. Mixed-use zoning along South Main Street would allow the school district's property to transition into mid-rise residential. Too much retail development would dilute the opportunity for existing businesses to profit. The facades along the street need immediate attention. Transit-oriented development is key to the village's long term success. The underpass which connects downtown to South Main is important because of connections to Lancaster's multi-modal station. Here there is a future connection to DFW and bus transfer operations. Also improvements on Lancaster, including lighting upgrades, support South Main's revitalization.

In the eastern part of the village, there are 40 acres of vacant and industrial land that could be redeveloped into recreational open space, residential and parks, and/or a use something like a stadium field surrounded by mixed-use properties. The consulting team presented Concept A highlighted by a youth sports complex and jogging trail. Concept B included residential without retail for mixed income persons who chose to rent or own. Open space, including public squares and parks, was encouraged in non-buildable areas. Concept C was the most dense, long term option presented and all planning evolved around a sports and entertainment outdoor stadium.

The attendees broke up into four groups and discussed their preferences for the concepts as well as brainstormed specific ideas for redevelopment. Overall, when each group's opinions were considered together, it was agreed that a short term catalyst could be the youth sports complex, with a focus on transit development. Among the ideas presented were:

- A light rail circulator system could move down South Main and Jennings Streets, similar to the way in which Portland has utilized a street car system to spur \$4 billion in private investment in seven years.
- Alternating parking lots along Main could help with vehicle access.
- Residential development is critical to support existing and future retail.
- A grocery store is needed.
- The transient and homeless populations must be addressed in order to alleviate safety concerns.

When presented with choosing between landscape and gateway improvements in the immediate future, everyone agreed that the \$500,000 in construction dollars would not go nearly far enough to make an impact. The group was somewhat divided as to whether the first gateway to be developed should be at South Main and Vickery or South Main and Pennsylvania. It was noted that a public art project is likely at South Main and Rosedale.

The next public meeting is scheduled for 10 a.m., Sept. 27, at Broadway Baptist Church.

SOUTH MAIN URBAN VILLAGE COMMUNITY MEETING SUMMARY - SEPTEMBER 27, 2007

Broadway Baptist Church 305 W. Broadway

Ann Kovich, with the TCB team, called the meeting to order and introduced members of the TCB and Village Advisory teams present for the third and final community meeting of the South Main Urban Village planning initiative.

In presenting an overview of the meeting approach, Mr. Ed Garza, with EDAW, emphasized the goals of the meeting: vision consensus, partnership framework, development opportunities, Main Street revival, practical solutions and transportation improvements. Mr. Garza noted that the village has more than \$700,000 in grant dollars for a transportation or landscape project to be consistent with the plan now being developed. Based on discussions during the first and second public meetings of the South Main Village, he reiterated the community's perceptions of the area's strengths and weaknesses. Top transportation/infrastructure improvement categories, as ranked by participants in the first public meeting, were street beautification, pedestrian improvements and vehicle access. Participants had preferred mixed-use zoning over conventional zoning examples.

To develop a framework, Mr. Garza established site analysis, historic inventory, existing zoning, demographics, opportunities and challenges of the area. The historic inventory evaluated six sites with a preliminary review, historic tax credit opportunities and Main Street core. Leland Consulting Group, a team member, assembled demographics for the South Main trade area, analyzing the distance people are willing to drive for services. The 2007 trade area demographics are compared against the city of Fort Worth and include a population of 36,908 with 12,968 households. The annual household growth rate (projected through 2030) is 1.5% with an average household size of 2.54 persons. Non-family households account for 43.4% and 55.3% are renters. The median household income is \$32,196, seniors aged 65 and older account for 12.5% and the African-American population is 29.8%.

Location to downtown, the west and south medical districts, transit-oriented development, availability of land and historic architecture are marketing opportunities within the trade area. Although the market analysis recognizes aged infrastructure, low socio-economic activity, homeless and transient populations, businesses that are not reliant on the local market can be challenged by a "dark and dangerous" perception, and lack of vehicular traffic.

The City of Fort Worth is beginning to implement the Mayor's Homeless Task Force as a proactive and positive solution to the 4,000 homeless. Within 10 years, the market demand will include up to 200 condo units, 300 rental housing units, 200,000 square feet of office space, 50,000 to 100,000 square feet of retail space and up to 150 rooms in a limited-service hotel. The level of investment which actually occurs, however, will be directly proportionate to the City and property owners' commitment to stronger physical connections, supportive infill policies, creative financial solutions and removal of barriers.

The overall urban village plan includes a Main Street core, school district property, residential housing, open space and transit oriented development. The proposed use plan will be a framework of incentives to encourage land use pattern (from T-5), expediting the review process and including criteria in NEZ for fee waiver and allowing for infrastructure upgrade financing. Marketing a branding strategy and organizing a liaison to shepherd the entire area will be important. Cooperative effort with city and other agencies for upgrades may include an incentive scale for partnership financing, waivers (not associated with NEZ) to lower cost to property owner, prioritized on CIP as "critical upgrades" and partnership with businesses to leverage funds. Mr. Garza stressed collaboration on financing options because funding will not come from a sole entity. New zoning will be equivalent to MU-2.

The action package includes utilizing a city-wide urban village fund of \$500,000 that is first come first serve. It requires a 50% match by the property owner and the maximum public amount is \$50,000 to be given as a grant or with low interest. A five-year property tax freeze may be included. Land banking is another action item along with establishing higher vacancy and increasing mobility, accessibility and safety. Full city services could be mobilized to do a clean sweep of the urban village and correct the top ten most serious code violations.

Capital improvements to the Main Street core are the number one priority. Other catalysts for redevelopment come from the Fort Worth ISD property and increased transit options. Residential availability can act as bookends to support mixed use amenities.

Ms. Kovich continued the meeting by outlining the existing conditions of transportation improvements, transportation alternatives and asking for preferences on funding recommendations. There are \$740,000 in total funds with \$500,000 available to spend.

She asked the participants to choose between two items:

- Item A outlined existing conditions of South Main's reputation as a "dark and dangerous" connection to downtown with lack of vehicular traffic to support commercial revitalization.
- Item B allowed for linear streetscaping and widening sidewalks along South Main.

It was noted a 12-inch waterline project is under discussion, but no funds have been identified at this time. Also under consideration is a public art component with \$50,000 available in a separate fund.

The participants favored Item A, the main Vickery intersection, due to concern over removing improvements in the future for the waterline project and that South Main's dark and dangerous reputation inhibits growth.

The meeting concluded with a description of Phase II of the initiative which will be the design effort. More public meetings will be held for input after approval by the City Council and Planning Commission in the next several months.